



CHICAGO PLAN COMMISSION

43 Green Phase II – Planned Development

East 43rd Street & S Prairie Ave

Grand Boulevard | 3rd Ward | Alderwoman Pat Dowell

43 Green JV LLC | The Habitat Company & P3 Markets

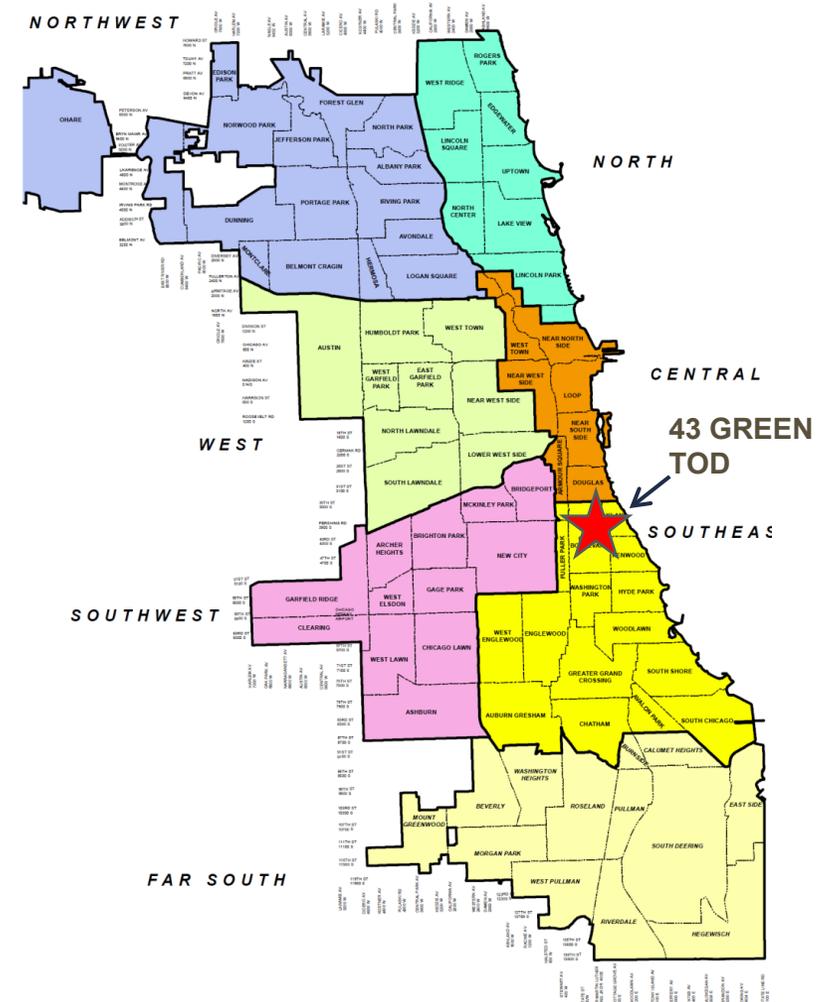
Landon Bone Baker | Moody Nolan

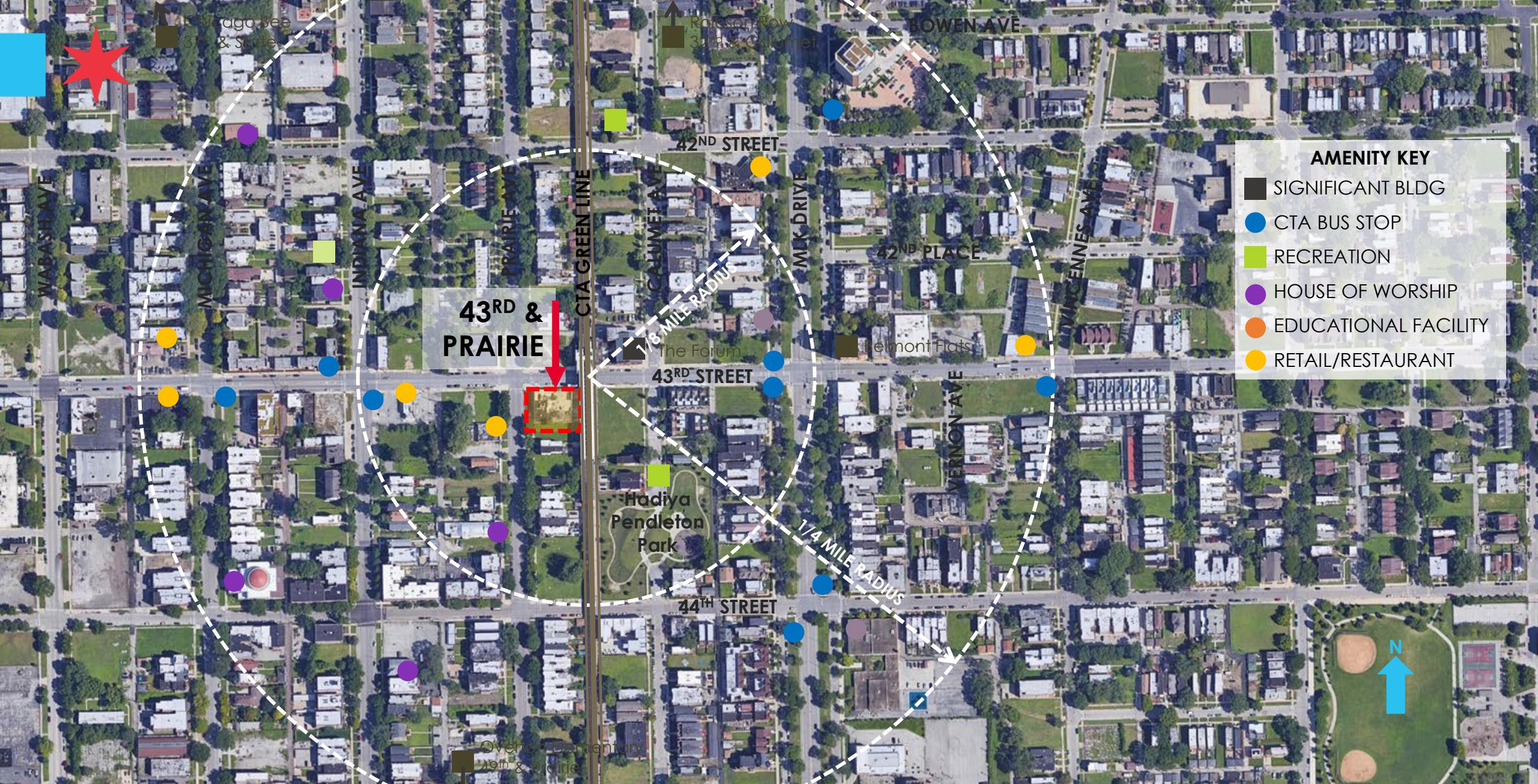
Steve D. Friedland | Applegate & Thorne-Thomsen

09/15/2022

Project Overview

- **43 Green Phase II** is a **transit-oriented, mixed-use commercial & residential development** anchored by the 43rd St. **Green Line** station at 43rd and Prairie.
- The development will directly **invest over \$100 million** in the historic **Bronzeville neighborhood** on Chicago's South Side.
- **10 Stories**
- **80 residential apartments**
- **Ground Level Retail** accessible from E. 43rd Street
- 13 Parking Spaces
- 56 Bike Parking Spaces
- **Total Proposed FAR is 4.2**

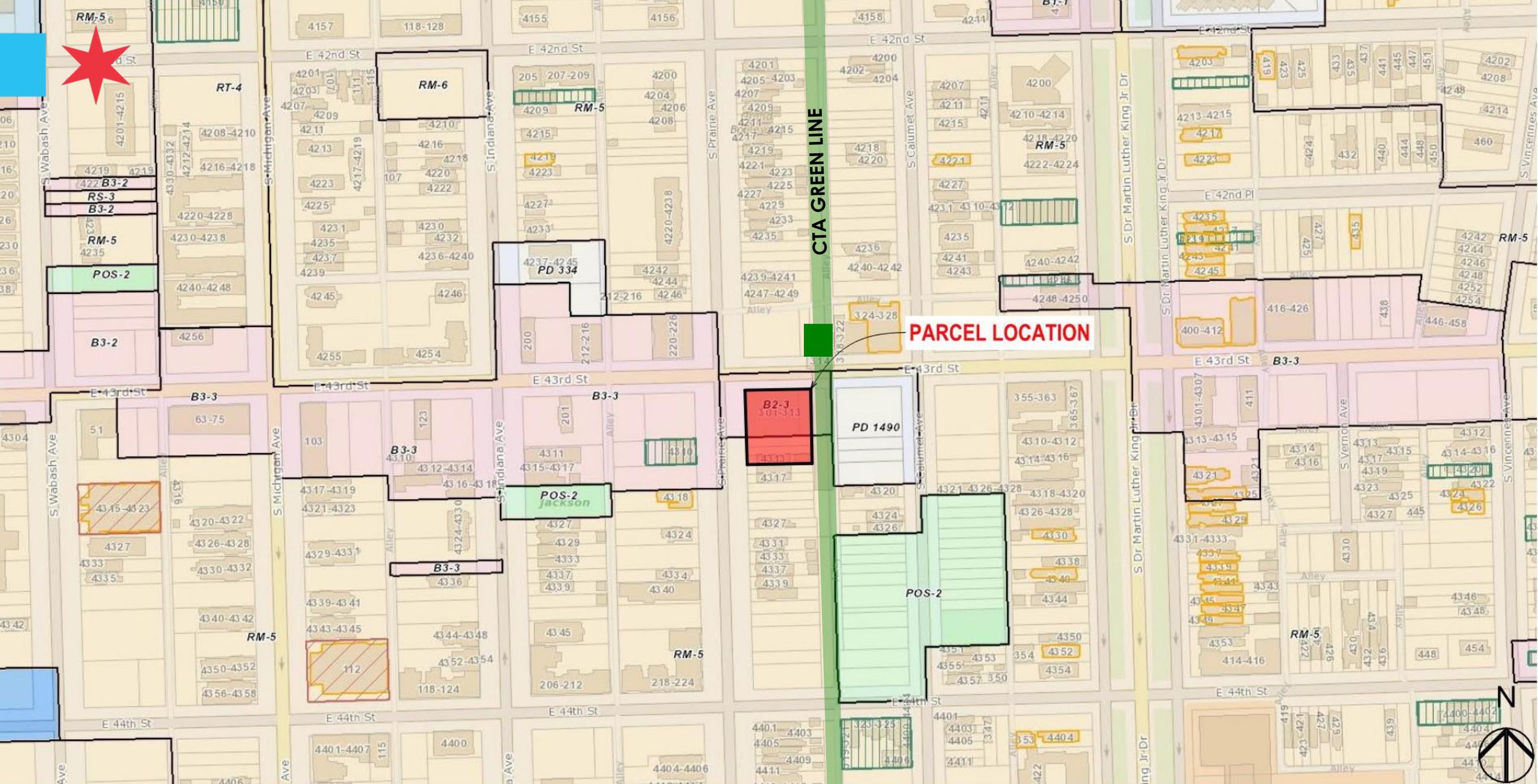




AMENITY KEY

- SIGNIFICANT BLDG
- CTA BUS STOP
- RECREATION
- HOUSE OF WORSHIP
- EDUCATIONAL FACILITY
- RETAIL/RESTAURANT

SITE CONTEXT PLAN



LAND USE CONTEXT PLAN



41ST & INDIANA
(11 STORIES)

41ST & MLK (NORTH)
(18 STORIES)

41ST & MLK (SOUTH)
(13 STORIES)

41ST & MLK (NORTH)
(26 STORIES)

41ST & VINCENNES
(11 STORIES)

43RD &
PRAIRIE

42ND & MLK
(20 STORIES)

43RD & MLK
(9 STORIES)

Phase I - 43RD &
CALUMET
(10 STORIES)

43RD STREET

PRAIRIE AVE

AERIAL VIEW FROM SOUTH

★ Development Team

The 43 Green project located in Bronzeville is more than a building -- it's a statement. A statement that what was once a vibrant retail corridor will rise again, a statement that brings people back to fill 43rd street sidewalks once again. A statement that Bronzeville is not just history, it's our future.

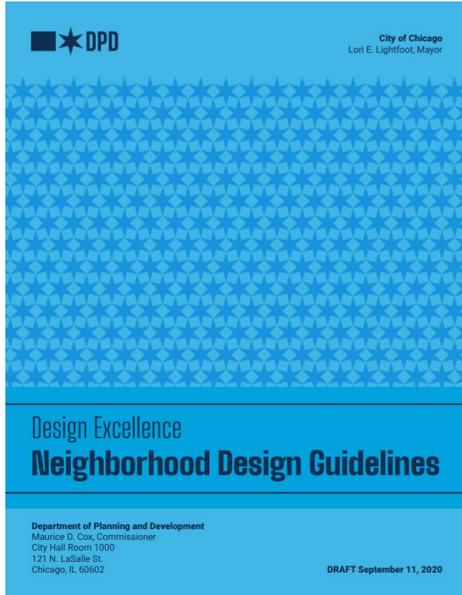
43 Green will begin the process of re-populating this corridor with young professionals that will bring a vibrancy that's been missing for decades. It will also continue the strong growth of single family homes by offering amenities such as new restaurants, clothing stores, financial institutions, and more.

When the 43 Green Development was introduced to the community November 2019, the response was positive and even received a standing ovation.

This project is more than a building, it's my neighborhood, my father's neighborhood, my grandfather's neighborhood and my children's neighborhood and my team and I appreciate your consideration.



★ Planning Context



Neighborhood Design Guidelines

City of Chicago Department of Planning and Development, September 2020

- Sustainability, Program, Site Design, Public Realm, Massing, and Façade



Bronzeville Retail District Land Use Plan

CMAP, The Bronzeville Alliance, Metropolitan Planning Council, August 2013

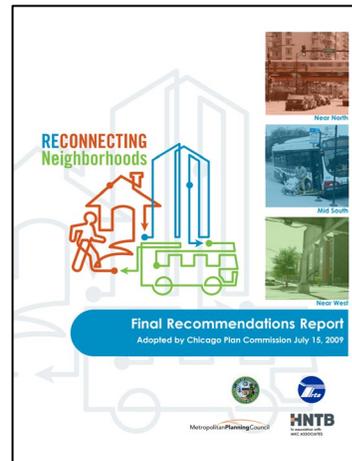
- Increase development density at transit nodes
- Rezone to create mixed-use commercial



Invest South/West

City of Chicago Department of Planning and Development, September 2020

- Priority Community



Reconnecting Neighborhoods

Chicago Plan Commission, July 2009

- Integrating Affordable Housing into the Neighborhood

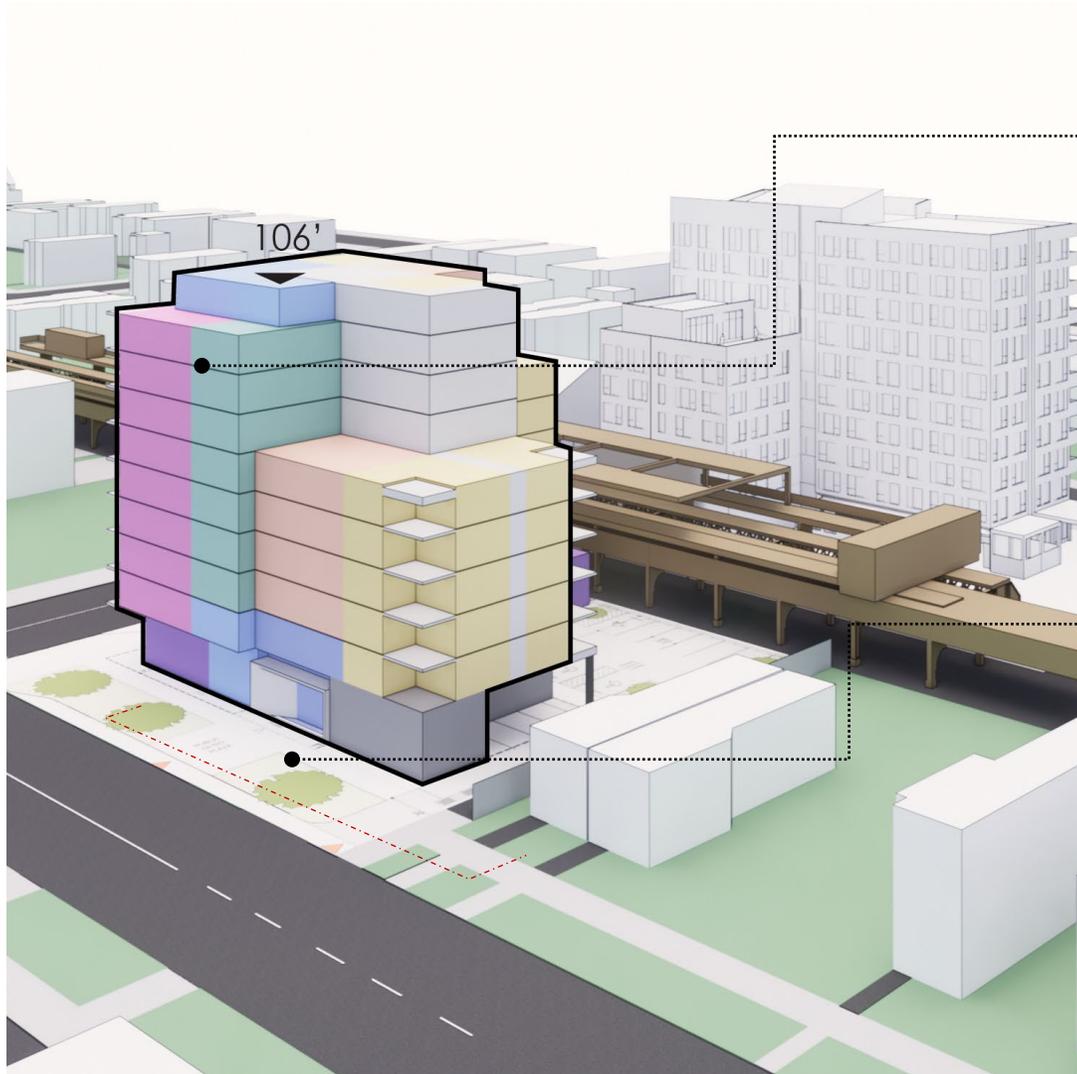


Design Excellence Principles

City of Chicago Department of Planning and Development, September 2020

- Equity, Innovation, Sense of Place, Sustainability, and Communication

★ Planning Context



- AMENITY
- 2 BR UNITS
- STUDIO A
- STUDIO B
- 1 BR UNITS
- COMMERCIAL
- BACK OF HOUSE
- RESIDENTIAL ENTRY

STUDIO: 24 units (30%)
 1-BEDROOM: 48 units (60%)
 2-BEDROOM: 8 units (10%)
TOTAL: 80 units
+/- 80,500 GSF

Neighborhood Design Guidelines

City of Chicago Department of Planning and Development, September 2020

- **Sustainable Design Expression** – Transit-oriented development with sustainable features throughout, including native plantings
- **Program** – Contextual mixed-use supporting commercial corridor and reinforcing residential
- **Site Design** – Priority given to pedestrian access and open space via terraces with views of the city; parking is screened from the street
- **Public Realm** – Ground floor is set back to accommodate a generous sidewalk along 43rd Street; commercial and residential entrances are distinct and welcoming
- **Massing** – Building steps down toward the residential neighbors to the South with the bulk facing 43rd maintaining the street wall but set back along Prairie
- **Façade** – High-quality materials where the scale of entrance treatment responds to the pedestrian experience

Community Engagement

Date of Community Meeting: November 11, 2019

- The project was **well-received** and **enthusiastically supported**
 - Interest in **preserving open space**
 - Support for inclusion of affordable and market rate units
 - Strong interest in local **Black-owned business development**
-
- Tax Credit Approval: December 2021

Development Goals:

- **Mixed income** affordable development
- Ground floor retail designed to **activate 43rd Street** and provide **services to the neighborhood**
- **Respecting** existing neighborhood buildings
- **High-quality** reasonably-priced apartments



Project Timeline

- **June 2021** – LIHTC Application Submission
- **December 2021** – LIHTC Funding Award
- **March 2022** – Committee on Design Presentation
- **April 2022** – PD Filing



LIHTC Application



CoD Presentation



Final Design

Diverse Design Team



NAYYAR & NAYYAR
International, Inc.
Structural Engineers



Pedestrian Context

RENDERING LOOKING SOUTH ALONG SOUTH PRARIE AVE



PHOTO OF EXISTING STREET CONTEXT



Pedestrian Context

RENDERING LOOKING NORTH ALONG SOUTH PRARIE AVE



PHOTO OF EXISTING STREET CONTEXT



Pedestrian Context

RENDERING LOOKING EAST FROM SOUTH PRAIRIE AVE

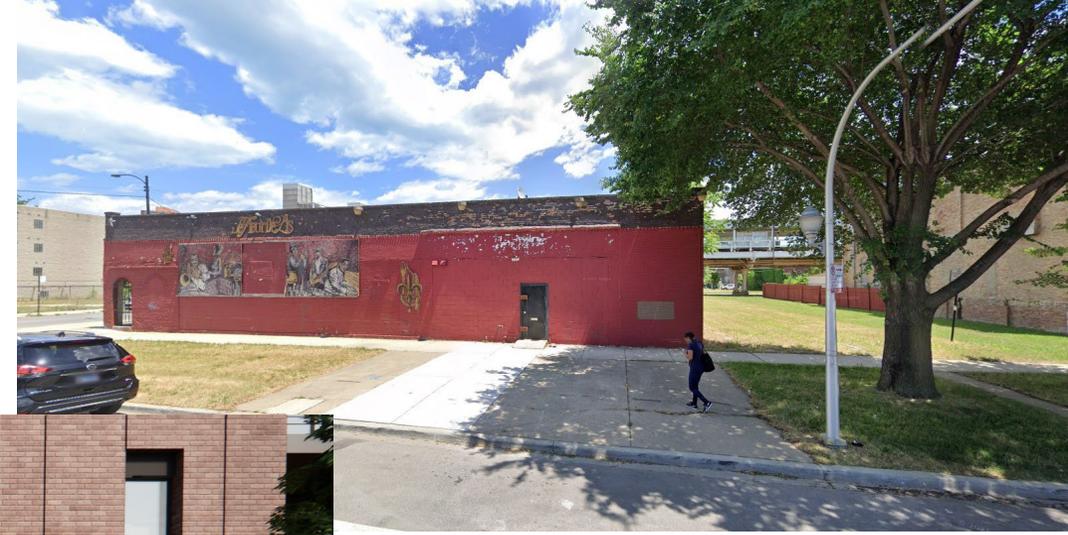
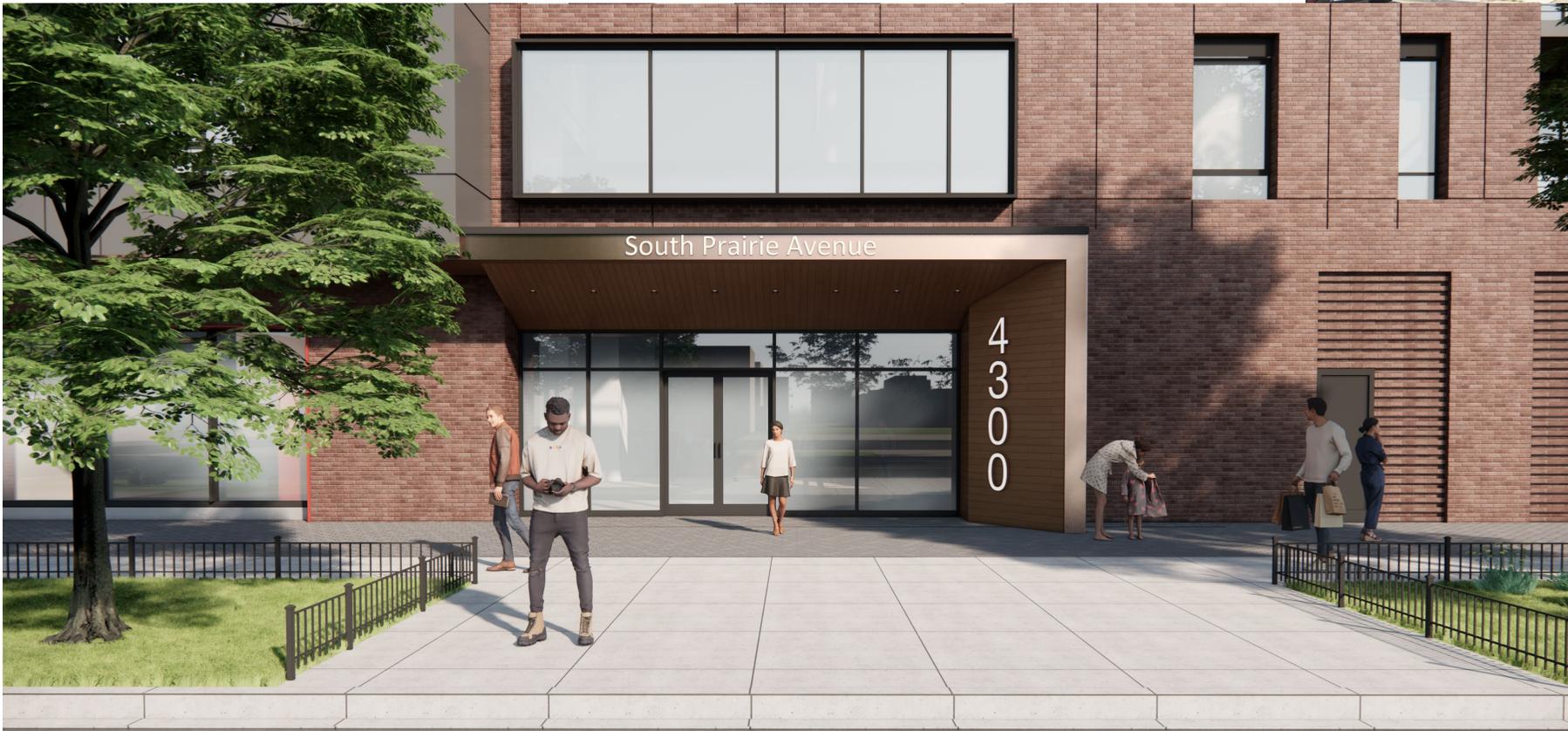
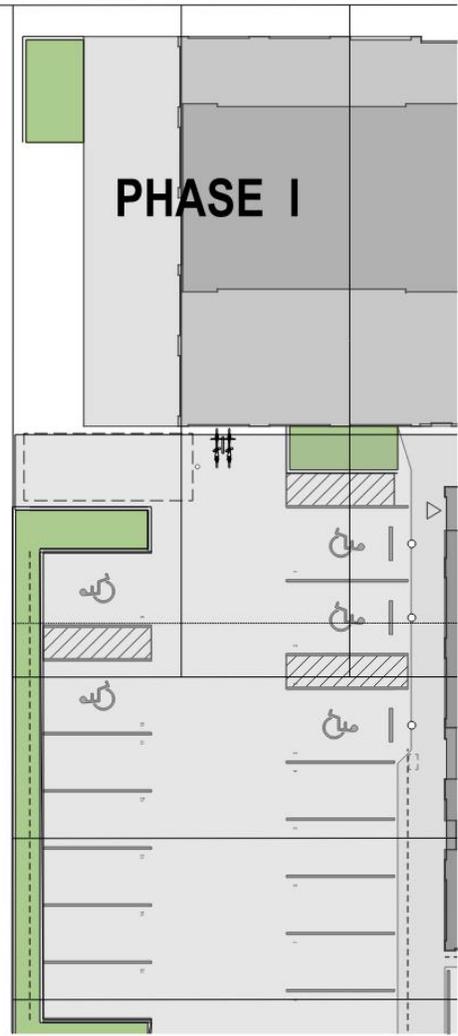
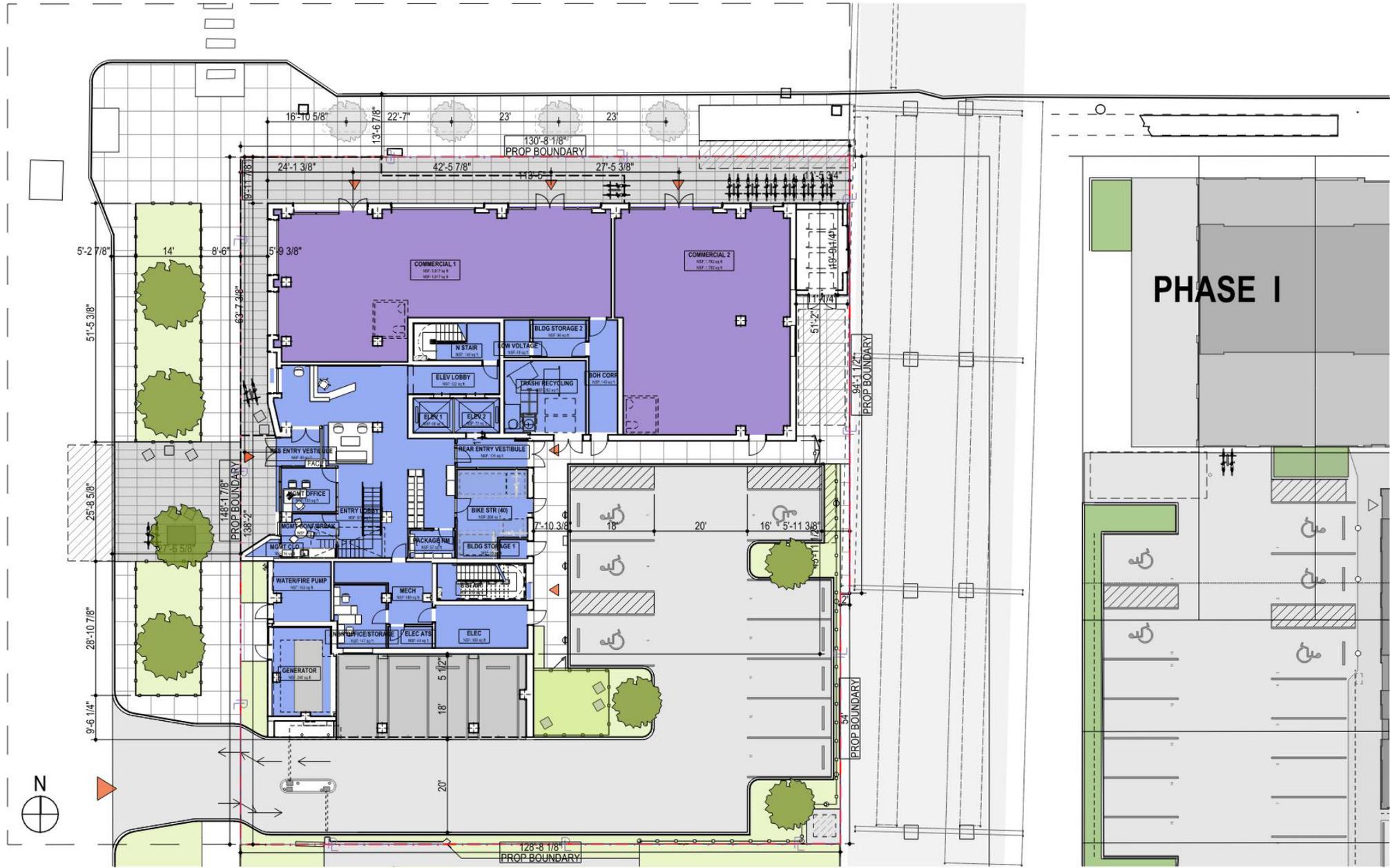


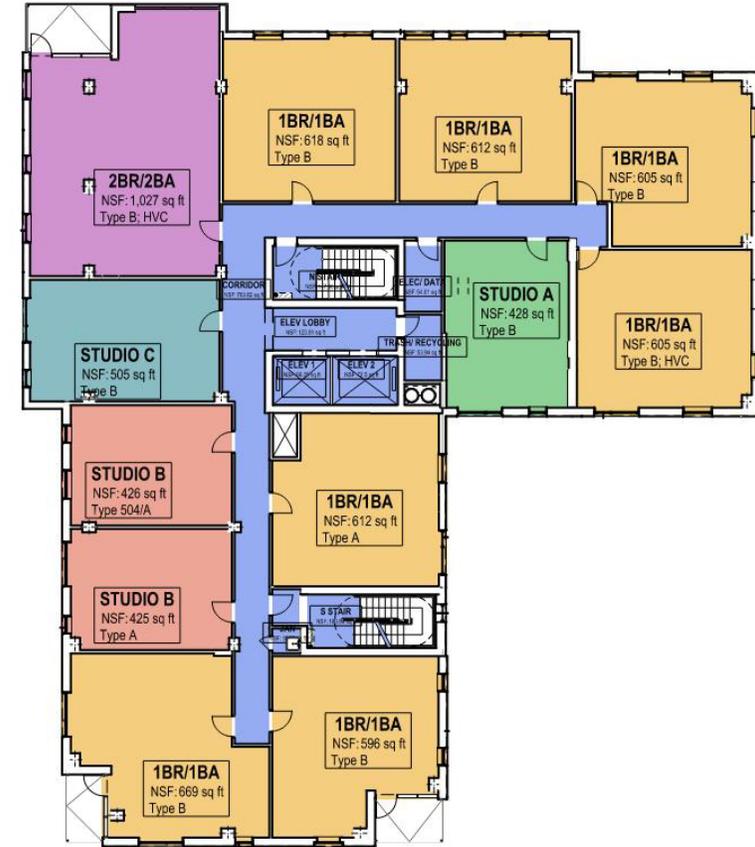
PHOTO OF EXISTING STREET CONTEXT



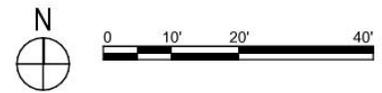
SITE + GROUND FLOOR PLAN

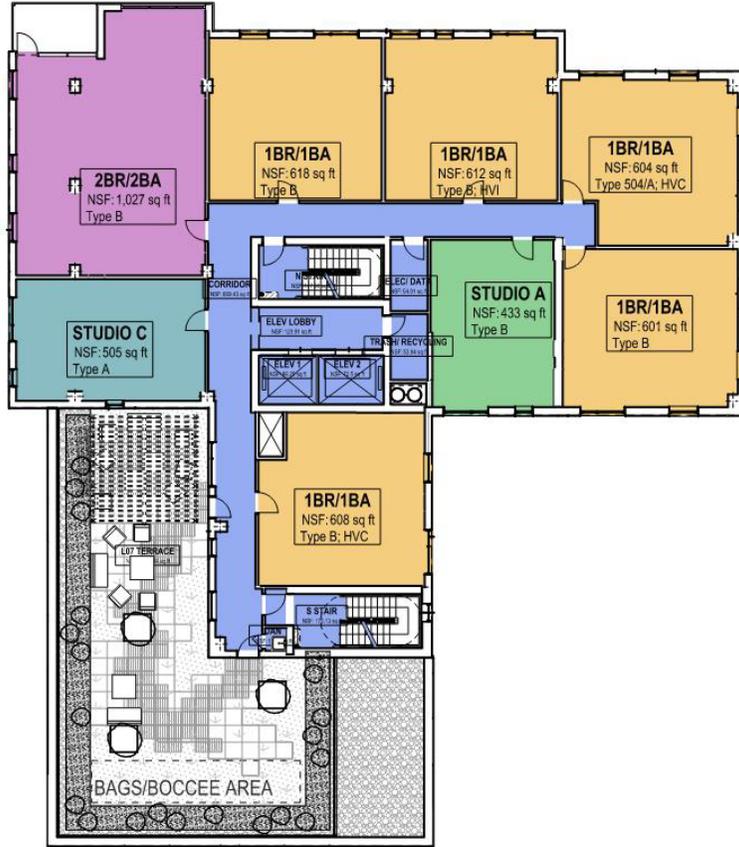


LEVEL 2

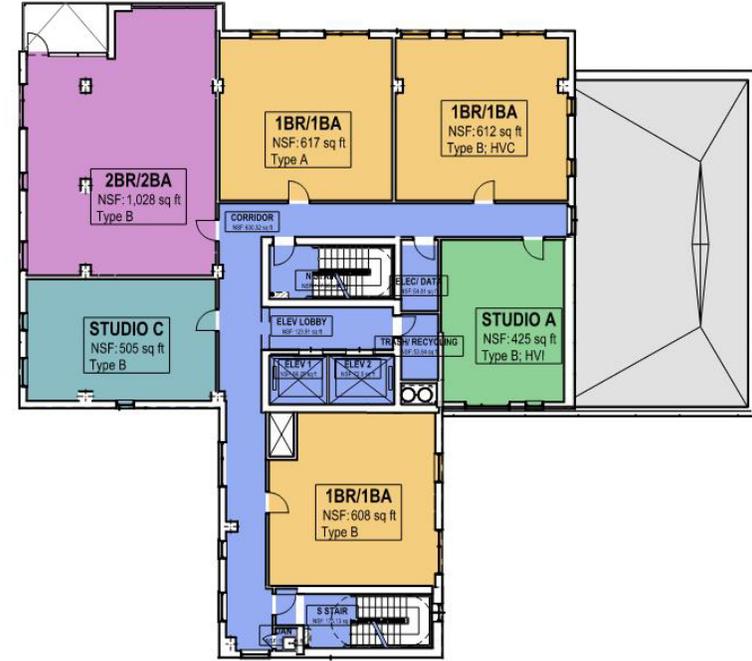


LEVEL 3-6

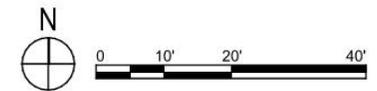


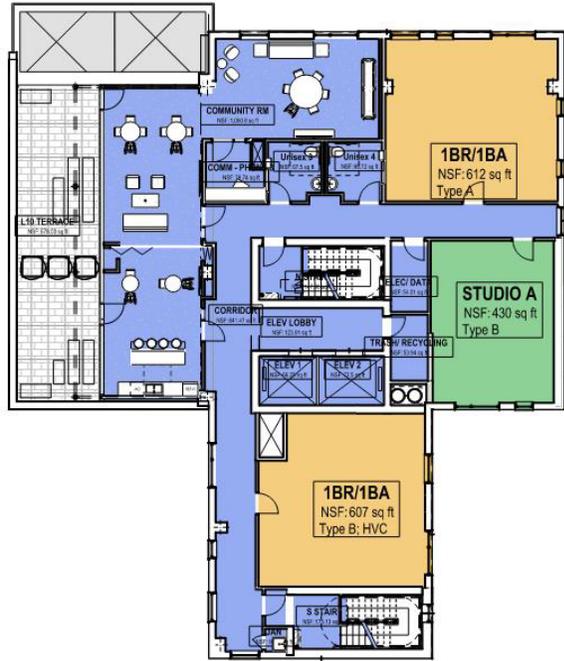


LEVEL 7

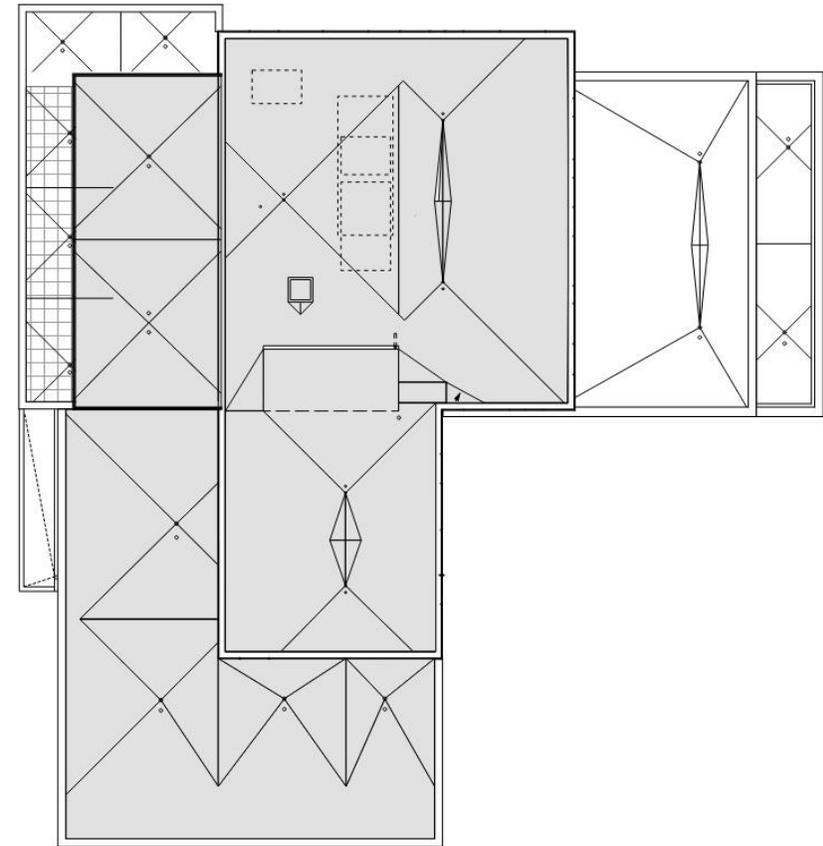


LEVEL 8-9

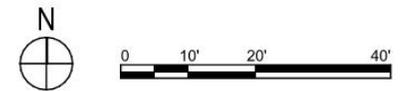




LEVEL 10



ROOF PLAN

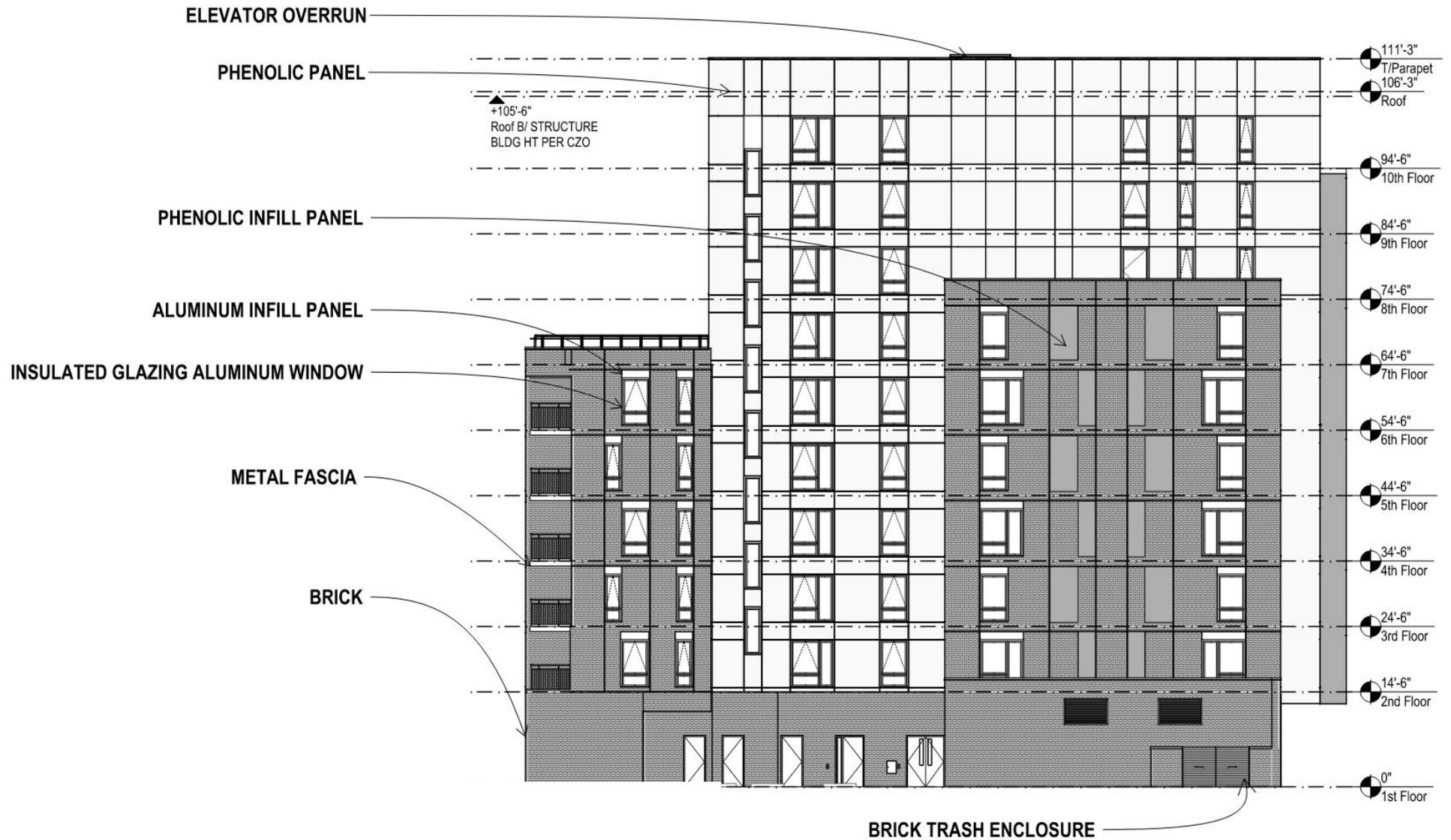




BUILDING ELEVATION - NORTH



BUILDING ELEVATION - SOUTH





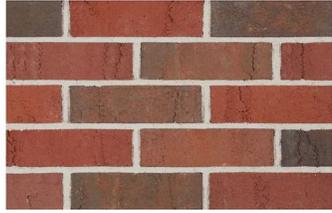
BUILDING ELEVATION - WEST



BUILDING SECTIONS

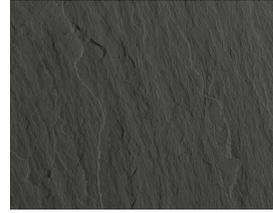
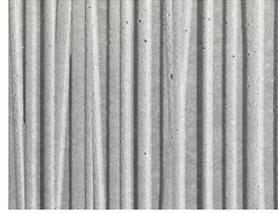


PREVIOUSLY PROPOSED BRICK



BERWICK BLEND
MODULAR BRICK

PREVIOUSLY PROPOSED FIBER REINFORCED CONCRETE PANELS



PROPOSED ALTERNATIVE BRICK



SIMULATED BELCREST 760
UTILITY BRICK

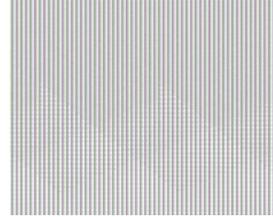
PROPOSED ALTERNATIVE PHENOLIC PANELS : TRESPA METEON UNI COLORS



LIGHT GREY
FINISH: ROCK



DARK GREY
FINISH: SATIN



RED ACCENT PANELS
FINISH: SATIN



PROPOSED ALUMINUM PLANK
SYSTEM - AL13
WOOD FINISH



ALUMINUM EXTRUSION
WINDOW FRAME AND PANEL:
DARK GREY
TBD BY AVAILABLE FINISHES



PROPOSED PHENOLIC ACCENT PANELS



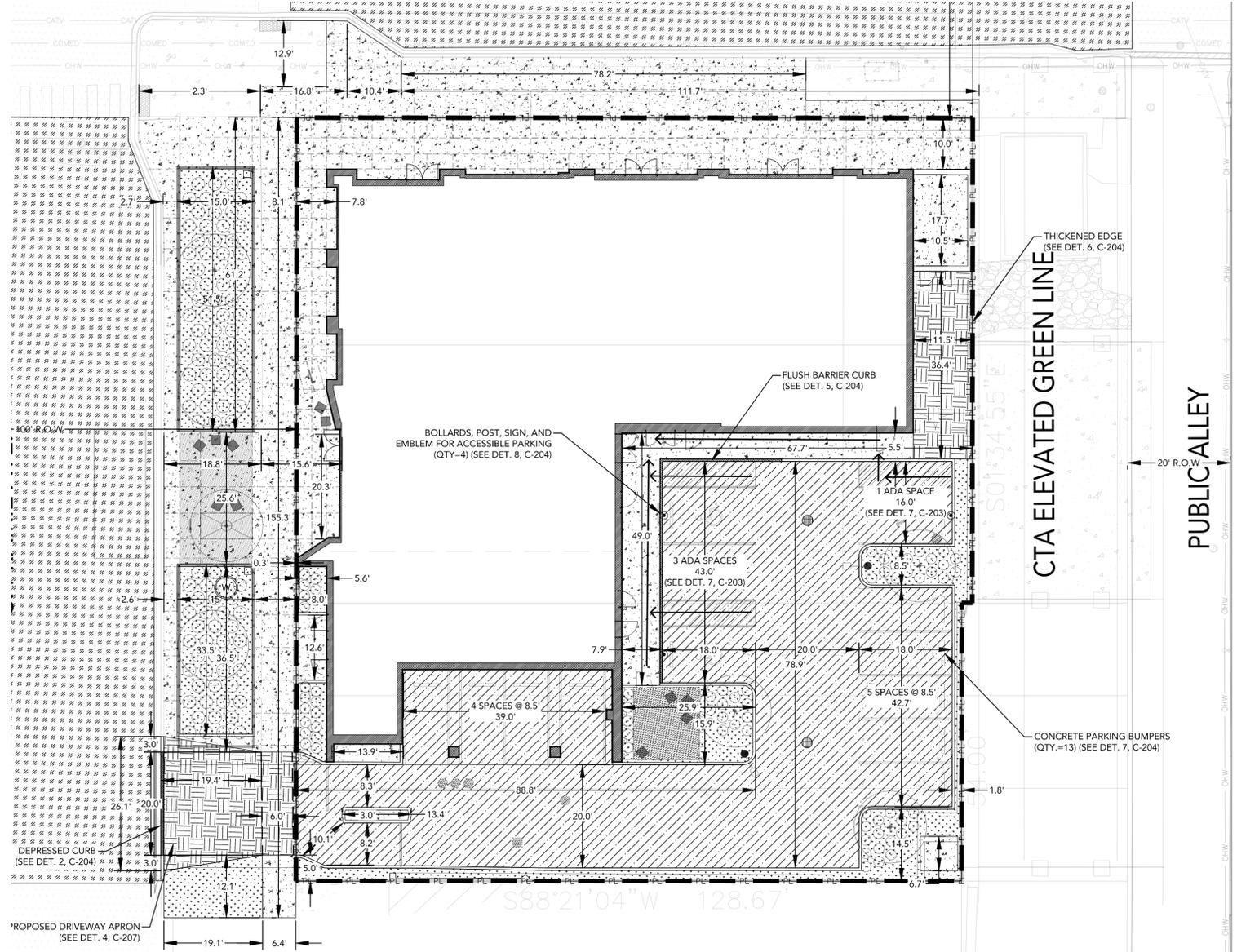
PROPOSED METAL PANEL AT LOBBY ENTRY
FINISH: WOOD



PROPOSED ALUMINUM EXTRUSION WINDOW FRAMES



- As depicted by arrows, accessible routes have been provided from all accessible spaces to the amenity areas and doors.
- Driveways are spaced adequately from adjacent neighbor to reduce traffic conflicts.
- Loading space has been provided in a separated location from the rest of the parking to reduce congestion.
- All streets and sidewalks follow typical City of Chicago standards.
- Driveway is the minimum allowable size to provide maximal pedestrian safety when crossing.

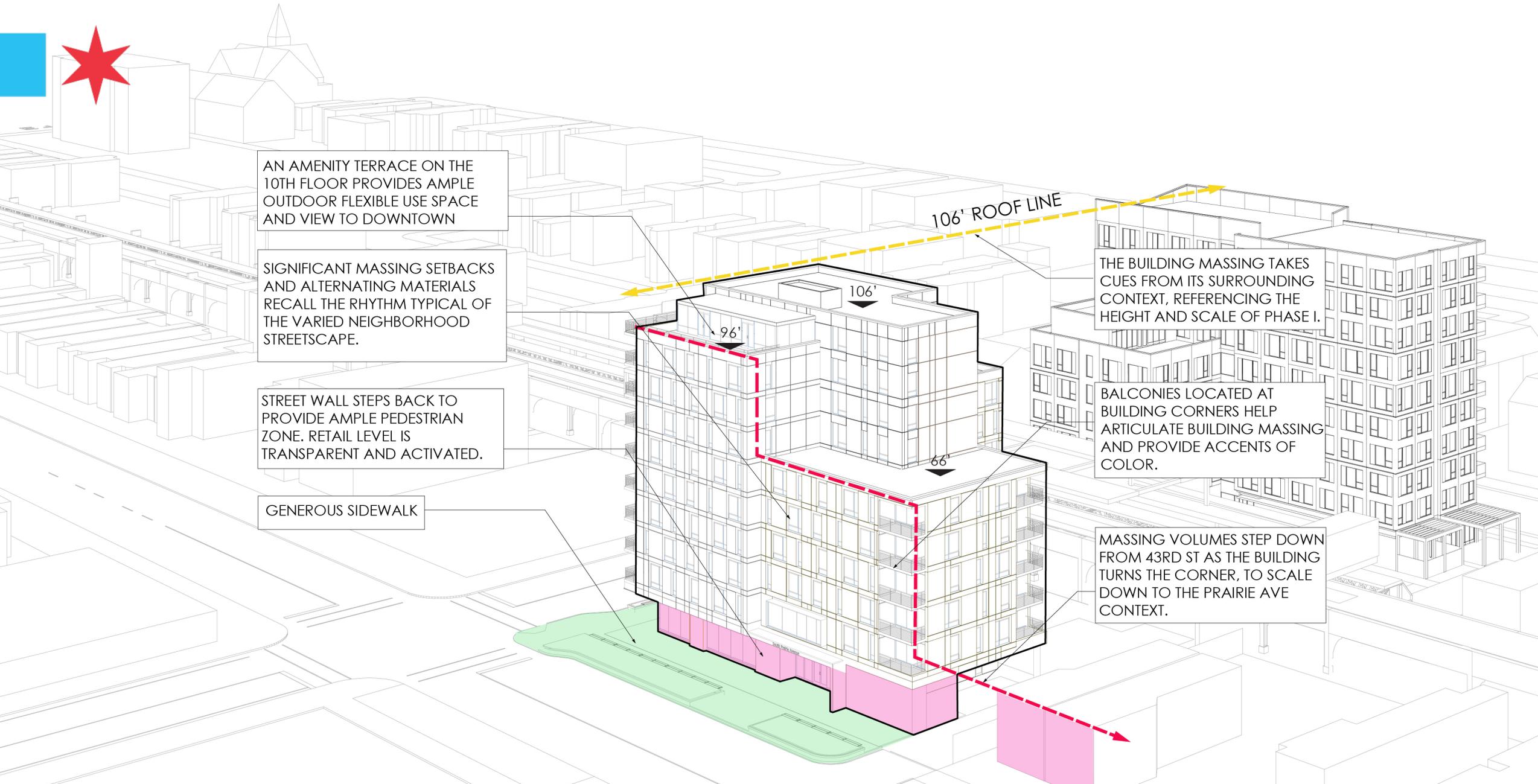


PROPOSED GEOMETRIC SHEET LEGEND

- CONCRETE WALKWAY
- LANDSCAPE
- HEAVY DUTY CONCRETE PAVEMENT
8" PORTLAND CEMENT CONCRETE
8" CA-6 AGGREGATE BASE COURSE
- CDOT PAVEMENT
RESURFACING/RESTORATION
- ASPHALT SURFACING (SEE
DET. 1, SHEET C-204)
- ADA ACCESSIBLE ROUTE

PARKING COUNT	
4	ADA SPACES PROVIDED
9	REGULAR SPACES PROVIDED
13	TOTAL SPACES PROVIDED

- NOTES:
- 1) CONTRACTOR SHALL VERIFY ALL DIMENSIONS IN FIELD PRIOR TO BEGINNING CONSTRUCTION. IF DIMENSIONS IN FIELD DO NOT APPEAR TO MATCH DIMENSIONS ON PLAN CONTACT CIVIL ENGINEER PRIOR TO PROCEEDING WITH CONSTRUCTION.
 - 2) DIMENSIONS ARE TO FACE-OF-CURBS AND FACE-OF-BUILDINGS UNLESS NOTED OTHERWISE.
 - 3) WORK CONCRETE WALKWAY PAVING AND JOINTING WITH DETAILS ON SHEET C-203 AND SPECIFICATION SECTION 32 16 13 "CONCRETE WALKS AND CURBS".



AN AMENITY TERRACE ON THE 10TH FLOOR PROVIDES AMPLE OUTDOOR FLEXIBLE USE SPACE AND VIEW TO DOWNTOWN

SIGNIFICANT MASSING SETBACKS AND ALTERNATING MATERIALS RECALL THE RHYTHM TYPICAL OF THE VARIED NEIGHBORHOOD STREETScape.

STREET WALL STEPS BACK TO PROVIDE AMPLE PEDESTRIAN ZONE. RETAIL LEVEL IS TRANSPARENT AND ACTIVATED.

GENEROUS SIDEWALK

106' ROOF LINE

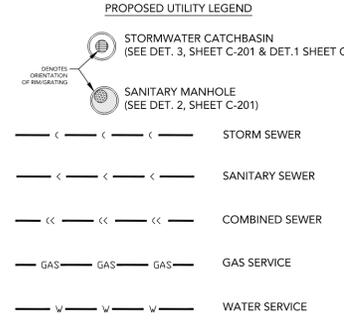
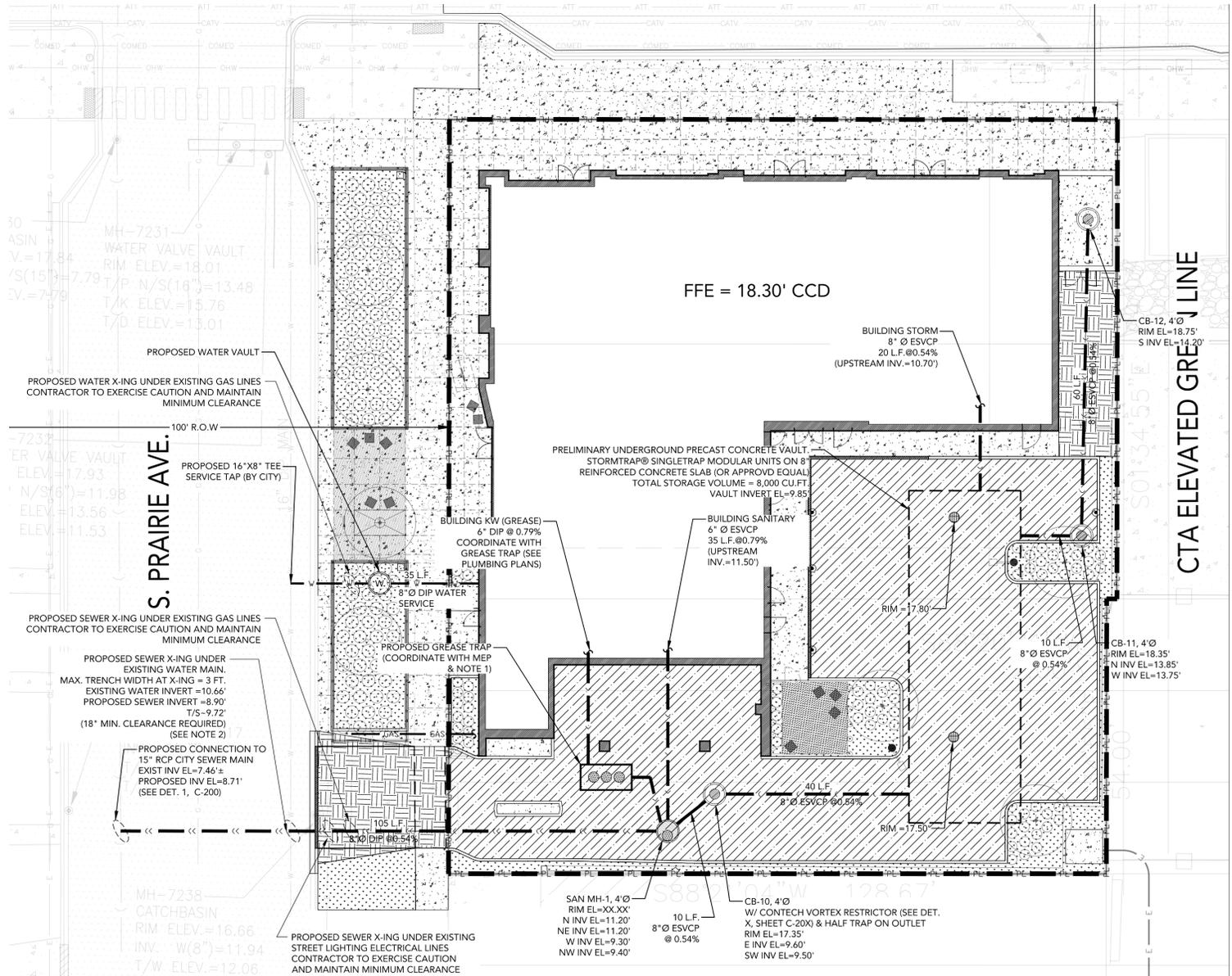
THE BUILDING MASSING TAKES CUES FROM ITS SURROUNDING CONTEXT, REFERENCING THE HEIGHT AND SCALE OF PHASE I.

BALCONIES LOCATED AT BUILDING CORNERS HELP ARTICULATE BUILDING MASSING AND PROVIDE ACCENTS OF COLOR.

MASSING VOLUMES STEP DOWN FROM 43RD ST AS THE BUILDING TURNS THE CORNER, TO SCALE DOWN TO THE PRAIRIE AVE CONTEXT.



- All stormwater on site, rate and volume control, will be managed within the detention vault located in the parking lot.
- The vault is sized to capture the rainwater from the 100-yr storm event.
- DWM Pre-intake meeting has already been completed and the initial stormwater approach has been reviewed.
- The ground level of the site is designed to drain at grade into the inlets located throughout the parking lot and into the storm system, as shown on the plan.



STORM WATER SUMMARY:
 100-YEAR DETENTION REQUIRED PER DWM STORMWATER TOOL = 7,025 CU.FT.
 OVERSIZED DETENTION REQUIRED = 7,820 CU.FT.
 STORAGE PROVIDED IN VAULT = 8,000 CU.FT.
 TOTAL STORAGE VOLUME PROVIDED = 8,000 CU.FT.
 VOLUME CONTROL MET BY OVERSIZED DETENTION

- NOTES:**
1. CONTRACTOR SHALL CONFIRM MINIMUM MANUFACTURER'S CLEARANCE BETWEEN THE PARKING CONTROL SYSTEM AND SURROUNDING INFRASTRUCTURE SUCH AS GREASE TRAP, SO AS TO NOT IMPEDE PROPER FUNCTIONING OF PARKING CONTROL SYSTEM.

STORMWATER MANAGEMENT ORDINANCE COMPLIANCE

Affordable Requirements Ordinance (ARO)

- Project will **exceed ARO requirements**
- **44 affordable** units (80 units total)
- 55% of units are affordable **at or below 60% AMI**
 - 37 at 60% AMI
 - 7 at 50% AMI



★ Community & Economic Benefits

- **Transit-oriented development**
- **Enhanced streetscape**
- **3700 SF commercial space**
- Project will satisfy the City's Participation Goals of:
 - 26% Participation from Qualified Minority Business Enterprises
 - 6% Participation from Qualified Women Business Enterprises
 - 50% Participation from Chicago Residents
- Business and Economics
 - **MBE Development Partnership**
 - **MBE-led General Contractor JV**
 - **MBE** engineering consultants
 - MBE retail/small **business opportunities**
 - Estimated **40 new permanent jobs**
 - Estimated **125 construction jobs**

\$45M total project cost

This is the 2nd phase of a \$100M investment in Bronzeville





DPD Recommendations

DPD has reviewed this proposal and determined that the project is consistent with the surrounding neighborhood's mix of residential and business uses and that it presents an opportunity to extend transit oriented, mixed-use development in the heart of the historic Bronzeville Community.

The Department has concluded that this proposal is appropriate for this site and supports this development for the following reasons:

1. The proposed rezoning of the subject property is appropriate under Chapter 17-13 of the Zoning Ordinance. The proposed planned development classification for the project is: 1) **appropriate because of growth and development trends** (17-13-0308-B); 2) The projects **uses, density and building scale are compatible with the character of the surrounding area** (17-13-0308-C); 3) **compatible with surrounding zoning districts** (17-13-0308-D)
2. **The project meets the purpose and criteria set forth in Chapter 17-8 of the Zoning Ordinance and its adoption would not have any adverse impact on the public's health, safety or welfare. Specifically, this project meets the following provisions of Chapter 17-8:**
 - a. **The project is in strict compliance with the underlying FAR standards of the B3-5 zoning designation** and is in substantial compliance with the other development control standards of the B3-5 zoning designation (17-8-901);
 - b. The project is consistent with the 47th and Kind Drive Tax Increment Financing District Plan, including the proposed use of the subject site (per 17-8-0903);
 - c. **All sides and areas of the building that are visible to the public are treated with materials, finishes and architectural details that are of high-quality and appropriate for use on primary street-facing façade** (per 17-8-0907-B3), as evidenced through the material callouts in this report and on the elevations;
 - d. Provides substantial landscaping of the open areas on the building and site (per 17-8-0909-A2) as demonstrated by the project's parking lot, parkway and building setback, area landscaping and trees.
3. **The proposed planned development promotes economically beneficial development patterns** that are compatible with the character of existing neighborhood (per 17-8-0103), as evidenced by the design of the project, and the fact that the proposed design and the proposed uses are in context with the character of the adjacent properties and will meet the needs of the immediate community;
4. **The proposed planned development promotes transit, pedestrian, and bicycle use by bringing density and retail activity to a site immediately adjacent to the 43rd Street Green Line Station.** The project ensures accessibility for persons with disabilities and minimizes conflicts with existing traffic patterns in the vicinity (per 17-8-0904-A- 1,2, 3 & 4). The project site plan works within the confines of the current existing traffic patterns at the site and creates little or no adverse impact to those patterns.

With these considerations the Department of Planning and Development recommends that this PD application be approved and forwarded to the City Council Committee on Zoning, Landmarks and Building Standards with passage recommended.