

File Copy

S. S. R. B.

JUSTIFICATION FOR NON-COMPETITIVE PROCUREMENT

DATE 12/02/08

APPROVED 3-0

~~CONDITIONALLY~~

~~APPROVED~~

~~RETURN TO DEPT~~

~~DISAPPROVED~~

COMPLETE THIS SECTION IF NEW CONTRACT(S)

For contract(s) in this request, answer applicable questions in each of the 4 major subject areas: Instructions for Preparation of Non-Competitive Procurement Form on the reverse side.

Request that negotiations be conducted only with OLD WORLD INDUSTRIES for the purchase of PGU described herein.
(Name of Person or Firm)

This is a request for: (One-Time Contract Per Requisition # , copy attached) or Term Agreement or Delegate Agency (Check one). If Delegate Agency, this request is for "blanket approval" of all contracts within the

 (Attach List) Pre-Assigned Specification No.
(Program Name) Pre-Assigned Contract No.

COMPLETE THIS SECTION IF AMENDMENT OR MODIFICATION TO CONTRACT

Describe in detail the change in terms of dollars, time period, scope of services, etc., its relationship to the original contract and the specific reasons for the change. Indicate both the original and the adjusted contract amount and/or expiration date with this change, as applicable. Attach copy of all supporting documents. Request approval for a contract amendment or modification to the following:

Contract #: 18624 Company, or Agency Name: OLD WORLD INDUSTRIES

Specification #: 69447 Contract or Program Description: LIQUID RUNWAY DEICER

Mod #: 41044 (Attach List, if multiple)

Al Perez 773-894-1823 Al Perez Aviation 12/2/08
Originator Name Telephone Signature Department Date

Indicate SEE ATTACHED in each box below if additional space needed:

(X) PROCUREMENT HISTORY

A Sole Source contract (PO#18624) for Propylene Glycol Urea (PGU) liquid runway deicer was recently awarded on November 14, 2008. That contract calls for the supply of 1,500,00 gallons of PGU to O'Hare International Airport to be delivered as required by the Department of Aviation (DOA). The contract allowed the contractor a minimum of 30 days after award to procure the needed raw materials and the required blending equipment. Originally it was hoped to award the contract by 11/1/08 and have access to PGU by 12/1/08, however a delay in award has the target date for delivery of PGU set back to the week of December 22, 2008. In the meantime while we await the PGU availability there is a concern that DOA's existing inventory may be depleted prior to then leaving the City with an inability to deicer its runways. This could jeopardize safety and lead to closure of runways and possibly the airport and the resulting chain reaction would impact air traffic across the USA and World. As such DOA would like to amend the contract to allow the contractor to provide potassium acetate (PA) in the interim if weather dictates the need to do so. If our current supply holds up we would not be committed to procuring the PA. Similarly, Old World would only be responsible for providing the product if they have it. They have the right to sell to others on a first come, first served basis.

The Sole Source was originally required as bids for PA has yielded no responsible bidders as the workers who mine one of this raw materials (Potash) were on strike. Although this strike has been recently settled, the other large manufacturers of PA, Clariant, Octagon Process and Cryotech have stated that they will not have PA for at least eight weeks and then only in limited quantities.

(X) ESTIMATED COST

\$11.49 gallon which is the same as the PGU price. Based on 350,000 gallons the potential value is \$4,021,500.

(X) SCHEDULE REQUIREMENTS

DOA requires immediate execution of the amendment to ensure its ability to secure an alternate deicer while we await the availability of PGU. The extended forecast for the next few weeks shows the potential for higher than average precipitation/snow.(see attached)

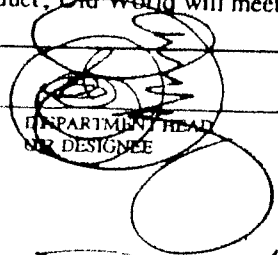
EXCLUSIVE OR UNIQUE CAPABILITY

Pursuant to the attached e-mails from Clariant, Octagon Process and Cryotech, those firms are unable to provide PA at this time. Old World currently has a small (approximately 350,000 gallons) inventory that they are making available on a first come, first served basis. Although Cryotech has their newly approved deicer NX360, ready for delivery, DOA is not yet comfortable using the product until it is used more widely and passes its long term storage test. The long term storage test results will not be in place until next year. In addition the pricing for this new product would be similar to PA and as we do not have a contract with Cryotech, the timeline for procurement is not favorable. Cryotech would have to figure out logistics, compliance etc and we would be at a point of using a Source product that we have no familiarity with.

OTHER

For this additional product, Old World will meet the MBE/WBE compliance commitment percentages of the original contract.

APPROVED BY:

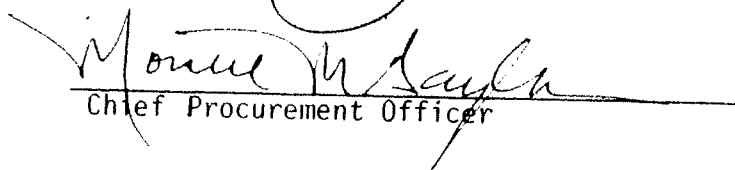


DEPARTMENT HEAD
OR DESIGNEE

12-20-08
DATE

2 5 4
BOARD CHAIRPERSON

12/02/08
DATE


Chief Procurement Officer

12-9-08
Date

INSTRUCTIONS FOR PREPARATION OF NON-COMPETITIVE PROCUREMENT FORM (Rev 9/9)

If a City Department has determined that the purchase of supplies, equipment, work and/or services can not be done on a competitive basis, a sole source justification must be prepared on this Justification for Non-Competitive Procurement Form in which procurement is requested on a non-bid or non-competitive basis in accordance with 65 ILCS 5/8-10-4 of the Illinois Compiled Statutes. All applicable questions in each Subject Area below must be answered. The information provided must be complete and in sufficient detail to allow for a decision to be made by the Non-Competitive Procurement Review Board. The Board will not consider justification with incomplete information or documentation. Also, attach Form P-7 (if One Time Contract); P-8 (if Delegate Agency Contract) or P-26 (if Term Agreement) to obtain a pre-assigned Specification and Contract Number for each contract in this request.

PROCUREMENT HISTORY (INCLUDING FUTURE PROCUREMENT OBJECTIVES)

1. Describe the requirement and how it evolved from initial planning to its present, status.
2. Is this a first time requirement or a continuation of previous procurement from the same source? If so, explain the procurement history.
3. Explain attempts made to competitively bid the requirement. (Attach copy of notices and list of sources contacted).
4. Describe any research done to find other sources (List other cities contacted, companies in the industry contacted, professional organizations, periodicals and other publications used).
5. Explain future procurement objectives. Is this a one-time request or will future requests be made for doing business with the same source?
6. Explain whether or not future competitive bidding is possible. If not, why not?

ESTIMATED COST

1. What is the estimated cost for this requirement (or for each contract, if multiple awards contemplated)? What is the funding source?
2. What is the estimated cost by fiscal year, if the job, project or program covers multiple years?
3. Explain the basis for estimating the cost and what assumptions were made and/or data used (ie. budgeted amount, previous contract price, current catalog or cost proposal from firms solicited, engineering or in-house estimate, etc).
4. Explain whether the proposed Contractor or the City has a substantial dollar investment in original design, tooling or other factors which would be duplicated at City expense if another source was considered. Describe cost savings or other measurable benefits to the City which may be achieved.
5. Explain what negotiation of price has occurred or will occur. Detail why the estimated cost is deemed reasonable.

SCHEDULE REQUIREMENTS

1. Explain how the schedule was developed and at what point the specific dates were known.
2. Is lack of drawings and/or specifications a constraining factor to competitive bidding? If so, why is the proposed Contractor the only person or firm able to perform under these circumstances? Why are the drawings and specifications lacking? What is the lead time required to get drawings and specifications suitable for competition? If lack of drawings and specifications is not a constraining factor to competitive bidding, explain why only one person or firm can meet the required schedule.
3. Outline the required schedule by delivery or completion dates and explain the reasons why the schedule is critical.
4. Describe in detail what impact delays for competitive bidding would have on City operations, programs, costs and budgeted funds.

EXCLUSIVE OR UNIQUE CAPABILITY

1. If contemplating hiring a person or firm as a Professional Service Consultant, explain in detail what professional skills, expertise, qualifications or other factors make this person or firm exclusively or uniquely qualified for the project. Attach copy of cost proposal and scope of services.
2. Does the proposed firm have personnel considered unquestionably predominant in the particular field?
3. What prior experience of a highly specialized nature does the person or firm exclusively possess that is vital to the job, project or program?
4. What technical facilities or test equipment does the person or firm exclusively possess of a highly specialized nature which is vital to the job?
5. What other capabilities and/or capacity does the proposed firm possess which is necessary for the specific job, project or program which makes them the only source who can perform the work within the required time schedule without unreasonable costs to the City?
6. If procuring products or equipment, describe the intended use and explain any exclusive or unique capabilities, features and/or functions the items have which no other brands or models, etc possess. Is compatibility with existing equipment critical from an operational standpoint? Explain why.
7. Is competition precluded because of the existence of patent rights, copyrights, trade secrets, technical data, or other proprietary data? Attach documentation verifying such.
8. If procuring replacement parts and/or maintenance services, explain whether or not replacement parts and/or services can be obtained from any other sources? If not, is the proposed firm the only authorized or exclusive dealer/distributor and/or service center? If so, attach letter from manufacturer.

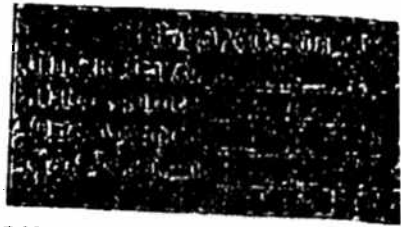
OTHER

1. Explain other related considerations and attach all applicable supporting documents (Information Technology Strategy Committee (ITSC) Approval form, etc.)
2. Explain what opportunities of direct/indirect involvement of Minority or Women Business Enterprises have been discussed and/or are available on this contract.

REVIEW AND APPROVAL

This form must be signed by both the Originator of the request and approved by the Department Head or, authorized designee.

DPS PROJECT CHECKLIST



IMPORTANT: PLEASE READ AND FOLLOW THE INSTRUCTIONS FOR COMPLETING THE PROJECT CHECKLIST AND CONTACT THE APPROPRIATE UNIT MANAGER IF YOU HAVE ANY FURTHER QUESTIONS. ALL INFORMATION SHOULD BE COMPLETED, ATTACH ALL REQUIRED MATERIALS AND SUBMIT FOR HANDLING TO THE DEPARTMENT OF PROCUREMENT SERVICES, ROOM 403, CITY HALL, 121 N. LASALLE STREET, CHICAGO, ILLINOIS 60602.

GENERAL INFORMATION:

Date: _____
 REQ No.: 41044
 Spec# 69447
 PO No.: (if known): 18624
 Modification No.: (if known): _____
 Project Description: Liquid Runway Deicer

Contact Person: William Palivos DAVE BOWMAN
 Tel: 686-7411 Fax: _____ E-mail: 686-708 @cityofchicago.org
 Project Manager: Al Perez
 Tel: ~~686-7411~~ 686-894-1823 Fax: _____ E-mail: @cityofchicago.org
 Previous PO No.: (if known): _____

FUNDING:

- City: Corporate Bond Enterprise Grant* Other
 State: IDOT/Transit IDOT/Highway Grant* Other
 Federal: FHWA FTA FAA Grant* Other

| LINE | FY | FUND | DEPT | ORGN | APPR | ACTV | OBJT | PROJECT | RPTG | \$ DOLLAR AMOUNT |
|------|----|------|------|------|------|------|------|---------|------|------------------|
| | 08 | 746 | 85 | 4005 | 0340 | | 0340 | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |
| | | | | | | | | | | |

IF GRANT FUNDED, A COPY OF THE APPROVED GRANT AND APPLICATION ARE REQUIRED and any other Terms and Conditions that may apply. Estimated Value \$ 4,026,500

SCOPE STATEMENT:

Attached is a Detailed Scope of Services and/or Specification
 Amendment to add potassium acetate to existing sale source contract. Increase value limit to \$ 21,256,500

IMPORTANT: THIS IS A CRITICAL PORTION OF YOUR SUBMITTAL. IN ORDER FOR DPS TO ACCEPT YOUR SUBMITTAL YOU MUST COMPLETE THE SPECIFIC SCOPE REQUIREMENTS AS SET FORTH IN THE SUPPLEMENTAL CHECKLIST FOR THAT UNIT.

The following is a general description of what should be included in a Scope of Services or Specification:
 A clear description of all anticipated services and products, including: time frame for completion, special qualifications of prospective vendors, special requirements or needs of the project, locations, anticipated participating user departments, citation of any applicable City ordinance or state/federal regulation or statute.

TYPE OF PROCUREMENT REQUESTED (check all that apply):

- NEW REQUEST**
 Blanket Agreement
 Standard Agreement
 Small Orders

- MOD/AMENDMENT**
 Time Extension
 Vendor Limit Increase
 Scope Change/Price Increase Additional Line Item(s)
 Other (specify): _____

FORMS: Requisition Special Approvals Non-Competitive Review Board (NCRB)

CONTRACT TERM: Requested Term (number of months): thru 3/31/2010

RE BID/SUBMITTAL REQUIREMENTS:

Requesting Pre Bid/Submittal Conference? Yes No Requesting Site Visit? Yes No

Contract Code: 1924612010

DPS PROJECT CHECKLIST

ARCHITECTURAL/ENGINEERING SUPPLEMENTAL CHECKLIST

Required Attachments: Scope of Services, including location, description of project, services required, deliverables, and other information as required

Risk Management

Will services be performed within 50 feet of CTA train or other railroad property? Yes No

Will services be performed on or near a waterway? Yes No

If applicable, Pre-Qualification Category No.

Category Description:

For Pre-Qualification Program, attach list of suggested firms to be solicited

Other Agency Concurrence Required: None State Federal Other (fill in)

AVIATION CONSTRUCTION SUPPLEMENTAL CHECKLIST

DOA sign-off for final design documents: Yes No

Required Attachments:

Copy of Draft Contract Documents and Detailed Specifications.

Risk Management:

Current Insurance Requirements prepared/approved by Risk Management: Yes No

Will work be performed within 50 feet of CTA or ATS structure or property? Yes No

Will work be performed airside? Yes No

*NOTE: Any non-construction Aviation request, complete the applicable section.

COMMODITIES SUPPLEMENTAL CHECKLIST

Required Attachments: Detailed Specifications (Scope of Services) including detailed description of the product, delivery location, user department contact, price escalation considerations, Bidder's qualification, contract term and extension options, Contractor's qualifications, citation of any applicable City/State/Federal statutes or regulations, citation of any applicable technical standards and Price Lists/Catalogs, technical drawings and other exhibits and attachments as appropriate.

If Modification request, please verify and provide the following:

Contractor's Name: Old World Industries

Contractor's Address: 4065 Commercial Ave
Northbrook, IL 60062

Contractor's e-mail Address:

Contractor's Phone Number: 847-559-2046

Contractor's Contact Person: Joel Saltzman

CONSTRUCTION SUPPLEMENTAL CHECKLIST

Required attachments:

Copy of Draft (80% Completion), Contract Documents and Detailed Specifications

Risk Management

Will services be performed within 50 feet of CTA train or other railroad property? Yes No

Will services be performed on or near a waterway? Yes No

DPS PROJECT CHECKLIST

VEHICLES/HEAVY EQUIPMENT SUPPLEMENTAL CHECKLIST

Required Attachments:

- Detailed Specifications including detailed description of the vehicle(s) or equipment, mounted equipment, if any, and options/accessories.
- Special Provisions (Delivery, Warranty, Manuals, Training, Additional Unit Purchase Options, Bid Submittal Information, etc.)
- Delivery Location(s)
- Technical Literature
- Drawings, if any
- Part Number List (Manufacturer; or Dealer; or Other Source:)
- Current Price List(s)/Catalog(s)
- Special Approval Form
- Exhibits and Attachments

If Modification request, please verify and provide the following:

Contractor's Name:

Contractor's Address:

Contractor's e-mail Address:

Contractor's Phone Number:

Contractor's Contact Person:

PROFESSIONAL SERVICES SUPPLEMENTAL CHECKLIST

- Detailed description of project listing obligations of each party.
- The Schedule of Compensation
- Deliverables
- Request for individual contract services (if applicable)
- The appropriate EPS form
- ITSC (approved by BIS)
- OBM (approved by Budget form/memo)
- Grant document attached

Attach any documentation indicating any previous purchase activity to assist in the procurement process

TELECOMMUNICATIONS AND UTILITIES SUPPLEMENTAL CHECKLIST

Required Attachments: Detailed Scope of Services/Specification which sets forth all of the anticipated services and products the user department wants provided, including time frame for completion, special qualifications of prospective vendors, special requirements or needs of the project, locations, anticipated participating user departments, citation of any applicable City ordinance or state/federal regulation or statute.

Has the project been reviewed by DGS? Yes No

Attach copy of DGS Recommendation; Reservation(s); or participate under current contract.

Does the project include software? Yes No

If yes, is signed ITSC form attached? Yes No

Does the location involve:

A public way? Yes No

Any concession in the City's facilities? Yes No

Is it anticipated City Council approval of the project or contract will be required?

Yes No

DPS PROJECT CHECKLIST

~~WORK SERVICES/FACILITY MAINTENANCE SUPPLEMENTAL CHECKLIST~~

Required Attachments: Detailed Specifications (Scope of Services) including detailed description of the work, locations (with supporting detail), user department contacts, work hours/days, laborer/supervisor mix, compensation and price escalation considerations, Bidder's qualification, contract term and extension options, Contractor's qualifications, ~~citation of any applicable City/State/Federal statutes or regulations, citation of any applicable technical standards and Price Lists/Catalogs, technical drawings and other exhibits and attachments as appropriate.~~

Risk Management:

Will services be performed within 50 feet (50') of CTA train or other railroad property? Yes No

Will services be performed on or near a waterway? Yes No

Will services require the handling of hazardous/bio-waste material? Yes No

Will services require the blocking of streets or sidewalks which may affect public safety? Yes No

If Modification or Amendment request, please verify and provide the following:

Contractor's Name:

Contractor's Address:

Contractor's e-mail Address:

Contractor's Phone Number:

Contractor's Contact Person:



DEPARTMENT OF AVIATION

MEMORANDUM

Date: December 2, 2008

To: Montel M. Gayles
Chief Procurement Officer

Attention: James McIsaac
Deputy Procurement Officer

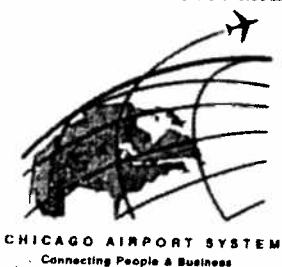
From: Richard L. Rodriguez
Commissioner

Subject: Request Approval for Amendment to
Non competitive Procurement Contract
Liquid Runway Deicer
Vendor: Old World Industries
PO Number: 18624
Specification Number: 69447
Expiration Date: 3/31/2010

The Department of Aviation (DOA) requests approval to modify the above referenced contract in order to allow for the ability to procure potassium acetate (PA) runway deicer. The subject contract for propylene glycol urea (PGU) was awarded in mid November and allowed the vendor a minimum of 30 days to procure the materials and equipment necessary to service the contract. Old World intends to be ready on or about December 20, 2008, however DOA is concerned its current inventory of runway deicer may not be sufficient to cover us until the PGU is available.

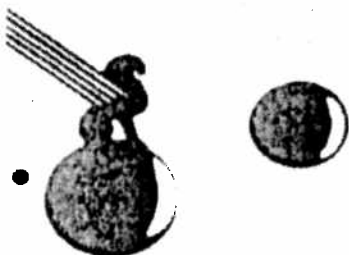
As there is no way to tell what the weather will be over the next three weeks (however long term forecasts predicts higher than average snow fall) DOA requires access to additional quantities of deicer to protect against inclement weather. We will only be obligated to purchase the product needed now and on an as available basis from Old World. Currently, Old World has approximately 350,000 gallons available.

If O'Hare were to run out of deicer it could jeopardize the safety of the traveling public and result in a runway/airport closure which would severely impact air traffic across the U.S. and entire World.



CHICAGO AIRPORT SYSTEM
Connecting People & Business





OLD WORLD INDUSTRIES, INC.
1065 COMMERCIAL AVENUE
NORTHBROOK, ILLINOIS 60062-1851
312-559-2000 • FAX 312-559-1329

December 1, 2005

Mr. Dave Bowman
City of Chicago
Department of Procurement Services
Room 403, City Hall
121 North LaSalle Street
Chicago, IL 60602

Re: Propylene Glycol Urea Runway Deicer Supply Agreement (the "Agreement") for
Chicago O'Hare International Airport ("O'Hare Airport") between the City of
Chicago (the "City") and Old World Industries, Inc. ("Old World")
Specification No.: 69447
P.O. No.: 18624
Vendor No.: 1050120

Dear Mr. Bowman:

Old World is pleased to submit the following non-binding proposal to the City relative to the above referenced Agreement. This proposal is in response to the City's request for access to an interim supply of runway deicer to supplement its current inventories until Old World is able to supply the runway deicer referenced in the Agreement.

This proposal would provide to O'Hare Airport (and Midway Airport if requested), first come-first served, subject-to-availability basis, varying quantities of Peak Potassium Acetate Runway Deicer Fluid ("PA Deicer") until Old World is able to manufacture and supply the ORD 2000 Propylene Glycol Urea Runway Deicer Fluid referenced in the Agreement.

This proposal is not intended to be and shall not be construed as a binding agreement. Any proposed obligations arising from this proposal shall be subject to and conditioned upon the execution, by and between the City and Old World of an actual amendment to the Agreement that is unconditionally acceptable to and accepted by both parties and executed by the appropriate authorized individuals.

In addition, this proposal and any amendment drafted there from is not intended to and shall not change the existing terms or conditions of the Agreement including Old World's MBE/WBE compliance commitments.

Mr. David Bowman
Department of Aviation
City of Chicago
December 1, 2008
Page 2 of 2

This proposal and any amendment drafted therefrom is also not intended to be and shall not be construed to be a minimum purchase obligation by the City for PA Deicer or a minimum sale and supply obligation by Old World of said product.

Proposed Non-Binding Commercial Terms:

Product: PEAK Potassium Acetate Runway Deicer (specification attached)

Price: \$11.49 per gallon, delivered to the Chicago-O'Hare International Airport or Chicago-Midway Airport

Payment Terms: Net 60 days from the date of invoice

Freight Terms: Freight to be prepaid by Old World

Lead Times: Old World would begin to load trucks no later than 24 hours after placement of product orders with deliveries to begin no later than 3 hours after initial pick-up


Quantity: Subject-to-availability on a first come first served basis

Term: Until supplies of PA Deicer are gone or until Old World terminates the availability of PA Deicer or until Old World is able to manufacture and supply the ORD 2000 Propylene Glycol Urea Runway Deicer referenced in the Agreement.

Thank you in advance for your consideration. Please contact me with any questions.

Sincerely,

Old World Industries, Inc.


Joel Salzman
Senior Vice President - General Manager
Chemicals

PEAK®

PA

LIQUID
POTASSIUM
ACETATE

Runway Deicer

Product Information Sheet



PEAK® PA Runway Deicing Fluid, a 50% (wt.) potassium acetate solution, is designed for airport applications. This liquid deicer is virtually odorless and contains a triazole-free corrosion inhibitor, so that it is 100% non-toxic. It is effective in extreme winter applications when temperatures are very low. PEAK® PA meets the stringent requirements of AMS 1435A.

DEICING

The primary purpose of chemical deicing is not to melt surface ice, but rather to diffuse down through the snow and ice to break the bond between frozen precipitation and the runway.
(typical use: 1 gal./1000 sq. ft.)

ANTI-ICING

The most cost effective way to utilize runway deicing fluid is by preventing frozen precipitation from sticking.
(typical use: 0.4 gal./1000 sq. ft.)

PREWETTING

When using solid deicer, such as PEAK® SF, better melting efficiency and adhesion occurs with liquid prewetting. (typical use: 70 parts solid to 30 parts liquid)

EQUIPMENT

Standard liquid spraying units can be used. Check with equipment manufacturer for exact recommendations.

Storage tanks are recommended to be of stainless steel, fiberglass, reinforced plastic or mild steel coated with protective liner. Clean and inspect equipment before and after the winter season.

Specifications

| | |
|---|---|
| APPEARANCE | Blue*, mobile liquid, free from suspended matter. |
| FREEZE POINT | Lower than -75°F (-60°C) |
| SPECIFIC GRAVITY | 1.265 - 1.285 @ 60°F |
| POUNDS PER GALLON, TYPICAL | 10.63 |
| pH | 9.3 - 10.3 |
| VISCOBITY | 20cP @ 32°F (0°C) |
| FLASH POINT (CC) | Nonflammable |
| SOLUBILITY IN WATER | Complete |
| BIODEGRADABILITY | >90% |

* Clear upon customer request

Tests show PEAK® PA Runway Deicing Fluid is readily biodegradable with low COD/BOD and aquatic toxicity. Always read and understand Material Safety Data Sheet before using the product.

- Active at sub-zero temperature (freeze pt. -75)
- Extended airport operation
- Improved safety during weather event
- Minimal environmental impact
- Cost effectiveness
- Sprayable liquid for ease of application
- Moderate pH range (9.3 - 10.3)



Old World Industries, Inc.
INDUSTRIAL CHEMICALS DIVISION

The Science of Deicing

4066 Commercial Avenue • Northbrook, Illinois 60062
Phone: 800-323-5440 • Fax: 847-559-2266
www.oldworldind.com/chemicals

080602

Thursday/Friday arctic blast; models generate big 2-week snow tallies here

Thursday/Friday arctic blast; models generate big 2-week snow tallies here

Sunday/Monday snowfall tallies

LAKE MICHIGAN WARMTH SPARES CITY BIG SNOW
If Lake Michigan warming hadn't changed snowfall to rain, Chicago would have been buried in snow!

HEAVIEST AREA TOTALS

| | |
|--|-------|
| Downers Grove | 5.7" |
| South Beloit | 5.5" |
| Huntley | 5.3" |
| Rockford | 4.6" |
| Westmont | 4.5" |
| Oak Brook | 4.4" |
| Elgin | 4.3" |
| OHARE | 1.09" |
| MIDWAY | 0.76" |
| Without lake's warming it could have been: | |
| 11.0" | 8.0" |

*Assuming a 10:1 snow-to-water ratio

SOURCES: Frank Wozniowski, National Weather Service archives

It's going to get windy Tuesday

ESTIMATED PEAK WIND GUSTS (M.P.H.)

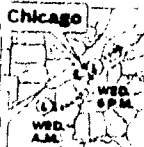


Wednesday into Thursday morning

CHICAGO'S NEXT STICKING SNOW?

Precipitation snapshot at 6 p.m. Wed.

Corridor of accumulating snowfall



Mammoth snow in the forecast

ESTIMATED 2-WEEK CHICAGO-AREA SNOW

Range among 7 most recent NWS* forecasts

LEAST 9.3" GREATEST 21.2"

AVERAGE OF 7 FORECASTS 14.6"

(NORMAL TALLY FOR PERIOD 3.4")

■ 6 of the 7 forecasts predict: 10"+

■ 4 of the 7 forecasts predict: 12"+

■ 3 of the 7 forecasts predict: 18"+

*National Weather Service's (local) Forecast System (OFS) model

Late-week arctic blast on the way

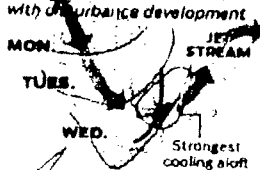
COLDEST AIR YET OF THE 2008-09 SEASON

Thursday 6 p.m. forecast temperature departure from normal



TRACKING THE COLD

Cold blast's arrival coincides with disturbance development



Evening positions

THOMAS VALLI, ERIC SZOB / WGN-TV

CLARIANT

704 904-0799

From: <Bryan.McCreary@clariant.com>
To: <DAVID.BOWMAN@cityofchicago.org>
Date: 12/2/2008 11:02:14 AM
Subject: Re: Deicer availability for 2008/09 snow season

Dave,

The strike at the mines owned by Potash Corp. of Saskatchewan was settled on Nov 13th. We were told through industry briefings and letters from the KOH manufacturers, that any KOH plants which had to convert to produce NaOH (sodium hydroxide) during the strike would require approximately 8-10 weeks to convert back to producing KOH and start supplying their customers. Of course, some plants may come back up sooner than others or take longer than others. While we may start to see some availability by years end, we are not expecting to have "production material" available until sometime in January, and even at that point we do not expect to be producing at full capacity and certainly nowhere near the volumes produced last year to support O'Hare. This limited capacity will most likely be sold on an "as available" basis as we would likely not pursue any "contract volume" commitments.

I hope this update helps with your planning. Please let me know what your current deicing fluid agreement is at O'Hare, should potassium acetate become available, i.e. are you commented to ORD 2000 for specific volumes or time frames.

regards,

Bryan

"DAVID BOWMAN"
<DAVID.BOWMAN@cityofchicago.org>
To
<Bryan.McCreary@clariant.com>
cc
Subject
12/01/2008 02:32 PM Re: Deicer availability for
2008/09 snow season

Bryan,

Per my phone message please advise on when you will be able to possibly begin providing limited quantities of PA. I believe last week you mentioned it would be at least 8-10 weeks until you would even have limited

capacity now that the mining strike is over.

This e-mail, and any attachments thereto, is intended only for use by the addressee(s) named herein and may contain legally privileged and/or confidential information. If you are not the intended recipient of this e-mail (or the person responsible for delivering this document to the intended recipient), you are hereby notified that any dissemination, distribution, printing or copying of this e-mail, and any attachment thereto, is strictly prohibited. If you have received this e-mail in error, please respond to the individual sending the message, and permanently delete the original and any copy of any e-mail and printout thereof.

From: "Munday, Amy" <amy.munday@cryotech.com>
To: <dbowman@ohare.com>
Date: 12/1/2008 1:34:11 PM
Subject: Potassium Acetate supply

Hello David,

As discussed, we are meeting with our potassium hydroxide (KOH) supplier later this week. KOH is a key raw material in the production of Cryotech E36 liquid runway deicer (potassium acetate). This meeting will give us a better understanding of when KOH will be available. With the information available today, we expect to begin producing E36 no earlier than February 2009.

In the interim, we do have Cryotech NX360 available and could ship to Chicago directly from Fort Madison, Iowa. I've attached a fact sheet and MSDS on NX360.

Please let me know if you would like additional information or have any questions.

Best Regards,

Amy Munday

Cryotech

1850 N. Clark St. - unit 405

Chicago, IL 60614

Ph: 312/951-1115

Cell: 515/770-2737

www.cryotech.com

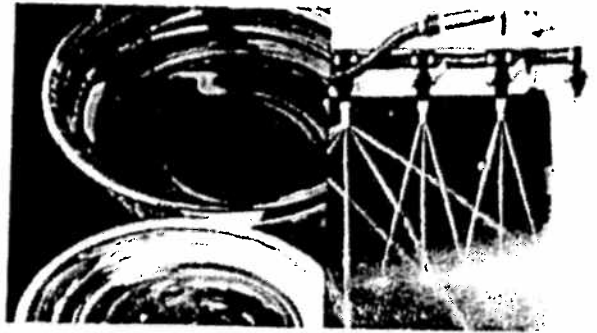


INNOVATIVE STRIDES IN CHALLENGING TIMES

Spec:

Cryotech NX360™

INNOVATIVE, NEW LIQUID RUNWAY DEICER
MADE WITH DUPONT TATE & LYLE SUSTERRA® PROPANEDIOL



Cryotech has developed a new deicer for the upcoming 2008-2009 winter season in response to the Potassium Acetate Deicer shortage. Cryotech NX360™ is a bio-based liquid runway deicer formulated for use where the outdoor air temperature (OAT) is 0°F (-18°C) or above on a routine basis. There are no anticipated differences in handling, pumping, and spraying between NX360 and Cryotech E36 at or above 0°F (-18°C). NX360 and E36 can be mixed at any ratio on pavement or in tanks. NX360 does not contain any corrosion inhibitors and is free of triazole (TTZ), ethoxylated compounds, nitrites, and nitrates.

PERFORMANCE

- Exhibits anti-icing characteristics as well as E36 (SHRP H-332)
- Active at low temperatures (0°F / -18°C)
- Meets required AMS 1435 specification

ENVIRONMENT

- Contains Susterra® propanediol, the bio-based component of NX360, which notably reduces energy use and greenhouse gas emissions over traditional propylene glycol formulations
- Readily biodegradable
- Recognized as "relatively harmless" by the U.S. Fish and Wildlife scale

APPLICATION

- Anti-icing: 0.5 gallons/1000ft² (25g/m²)
- Deicing: 1 - 3 gallons/1000ft² (50 g/m²) near 32°F (0°C) on thin ice
Below 15°F (-9°C), NX360 must be used in conjunction with a solid deicer such as Cryotech NAAC®
- Prewetting: apply using a rate at the spreader outlet of 1.25 gallons per 100 lbs (130g/kg) of solid deicer
- Re-apply when new accumulation shows first tendency to bond
- Plow often to reduce fluid dilution
- Easy to apply with existing equipment

STORAGE AND HANDLING

- NX360 should not be stored or plumbed through systems that use galvanized, zinc, or brass components
- If temperatures fall below 0°F (-18°C) on a routine basis, special storage and handling may be needed
- Polyethylene containers are preferred, otherwise use carbon, low alloy, or stainless steel containers
- Best practices include storing totes and drums in areas protected from weather and exposure to direct sunlight
- Containers must be clean and free of rust, surface deposits, and residues

ISO 9001
CERTIFIED



FM 39092

ISO 14001
CERTIFIED



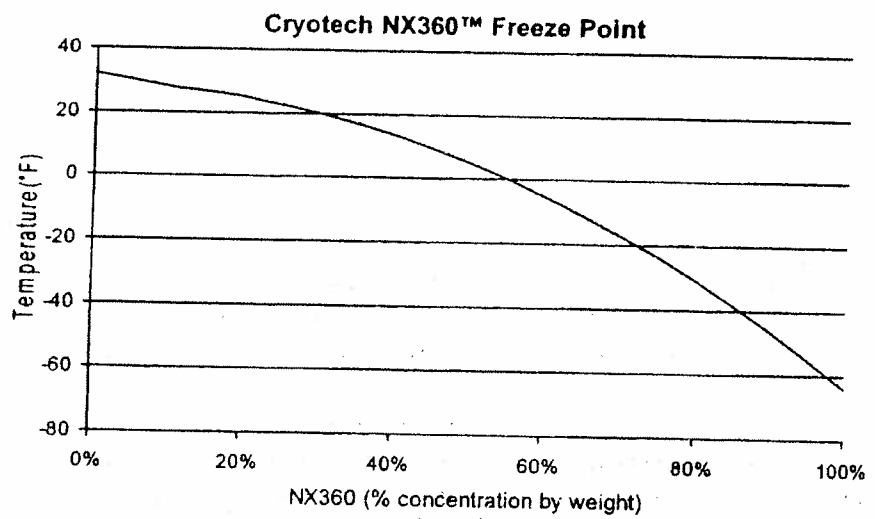
EMS 89384

See Reverse Side For Product Specifications

AN ISO 9001:2000 & 14001:2004 CERTIFIED COMPANY

| | |
|------------------------|---|
| COMPOSITION | Aqueous solution, |
| APPEARANCE | containing sodium acetate and Susterra® propanediol Clear*, mobile liquid, free from matter in suspension |
| DENSITY | At 68°F (20°C) = 9.25 lbs/gal (1.11 g/cm ³) |
| VISCOSITY | At 68°F (20°C) = 20cp maximum At 32°F (0°C) = 70cp maximum |
| FLASH POINT | Nonflammable |
| FREEZING POINT | <-60°F (-51°C)** |
| MISCIBILITY WITH WATER | Complete |
| TYPICAL pH | 9 to 11 |
| SPECIFIC GRAVITY | At 68°F (20°C) = 1.08 to 1.13 |
| PACKAGING | 55 gallon drums (208 liters) - (4 minimum) 265 gallon tote (1003 liters) - (1 minimum) Bulk (4400 gallons minimum) *MAY BE DYED BLUE AT CUSTOMER REQUEST |

**CRYOTECH
NX360™
FREEZE
POINT
CHART**



Revised - 10/30/08

TO ORDER OR FOR PRODUCT INFORMATION CONTACT:
 Ph: +1 319.372.6012 or +1 800.346.7237 Fax: +1 319.372.2662 E-mail: deicers@cryotech.com

CRYOTECH NX360™

MATERIAL SAFETY DATA SHEET



Cryotech NX360™ Liquid Runway Delcer
with DuPont Tate & Lyle Susterra™ propanediol
Complies with Specification AMS 1435
MANUFACTURED AND SUPPLIED IN THE USA BY
Cryotech Deicing Technology
6103 Orthoway
Fort Madison, IA 52627
United States

Cryotech Contact Information
Telephone: (800)346-7237
FAX: (319)372-2662
email: delcers@cryotech.com
website: <http://www.cryotech.com>

3. CHEMICAL COMPOSITION
The percent compositions are given to allow for the various ranges of the components present in the whole product and may not equal 100%.

| Percent | Component | CAS# |
|---------|--------------------------------|----------|
| 100% | Cryotech NX360 | |
| | An aqueous solution containing | |
| | Sodium Acetate | 127-09-3 |
| | 1,3-propanediol | 504-63-2 |

CAS - Chemical Abstract Service Number

4. PHYSICAL AND CHEMICAL PROPERTIES
(also see Sections 11 and 12)

EYE CONTACT:
This substance is slightly irritating to the eyes and could cause prolonged (days) impairment of your vision. The degree of the injury will depend on the amount of material that gets into the eye and the speed and thoroughness of the first aid treatment. Signs and symptoms may include pain, tears, swelling, redness and blurred vision.

SKIN IRRITATION:
This substance is not expected to cause prolonged or significant skin irritation.

DERMAL TOXICITY:
The systematic toxicity of this substance has not been determined. However, it should be practically non-toxic to internal organs if it gets on the skin.

RESPIRATORY/INHALATION:
This material does not present an inhalation hazard.

INGESTION:
If swallowed, this substance is considered practically non-toxic to internal organs. Ingestion may cause irritation of the digestive tract which may result in nausea, vomiting and diarrhea.

OCCUPATIONAL EXPOSURE LIMITS:
None Identified

5. FIRE AND FLAME HAZARDS
Chemical Emergency: Spill, leak, fire, or accident call
Chemtrec day or night (800)424-9300;
Outside continental USA call (703)527-3887

EYE CONTACT:
Flush eyes immediately with fresh water for at least 15 minutes while holding the eyelids open. Remove contact lenses if worn. No additional first aid should be necessary. However, if irritation persists, see a doctor.

SKIN CONTACT:
No first aid procedures are required. As a precaution, wash skin thoroughly with soap and water. Remove and wash contaminated clothing.

INHALATION:
Since this material is not expected to be an immediate inhalation problem, no first aid procedures are required.

INGESTION:
If swallowed, give water or milk to drink and telephone for medical advice. DO NOT make the person vomit unless directed to do so by medical personnel. If medical advice cannot be obtained, then take the person and product container to the nearest medical emergency treatment center or hospital.

5. FIRE FIGHTING MEASURES

FLASH POINT: NA
AUTO IGNITION: NA
FLAMMABILITY LIMITS (% by volume in air):
Lower: NA Upper: NA Non-flammable

EXTINGUISHING MEDIA:
NA - Material is not flammable

FIRE FIGHTING PROCEDURES:
This material normally will not burn.

COMBUSTION PRODUCTS:
Normal combustion forms carbon dioxide and water vapor.

NFPA RATINGS:
Health 1; Flammability 0; Reactivity 0; Special NDA:
(Least - 0, Slight - 1, Moderate - 2, High - 3, Extreme - 4)
These values are obtained using the guidelines or published evaluations prepared by the National Fire Protection Association (NFPA) or the National Paint Coating Association.

6. ACCIDENTAL RELEASE MEASURES

Chemical Emergency: Spill, leak, fire, or accident call
Outside continental USA call (703)527-3887
Chemtrec day or night (800)424-9300;

Contain spillage and absorb on suitable material e.g. sawdust, sand or earth. Transfer to a container for disposal. See section 13.
Wash the spillage area with plenty of water.

7. HANDLING AND STORAGE

Avoid contact with skin and eyes.
Avoid breathing mists when spraying.
Store in clean vessels and containers.
Do not store or handle product with systems constructed of wetted parts that have galvanized steel, zinc or brass components.

CRYOTECH NX360™

MATERIAL SAFETY DATA SHEET

| | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|--|--|-----|--------------------------------------|----|-----------------|----|---------------------------------------|----|-----------------------|----|---------------|---------------|---------------------|------------------|-------------|-------------------|--------------------|--------------------|-------------------|-------------|-------------|-------------------|-------------|----------------|-----------------|---------------------------|--------------|----------------|---------------------|---------------------|---------------------------|------------------|----------------------|---------------------|---------------------|---------------------|-------------------|--|---|
| <p>1. IDENTIFICATION:</p> <p>1.1. PRODUCT USE:</p> <p>EYE PROTECTION: Do not get this material in your eyes. Eye contact can be avoided by wearing chemical goggles.</p> <p>SKIN PROTECTION: No special skin protection is usually necessary. Avoid prolonged or frequently repeated skin contact with this material. Skin contact can be minimized by wearing protective clothing.</p> <p>RESPIRATORY PROTECTION: No special respiratory protection is normally required.</p> <p>VENTILATION: No special ventilation is necessary.</p> | <p>1.2. PHYSICAL AND CHEMICAL INFORMATION:</p> <p>COD (TOD): 0.92 kg O₂/kg BOD₅ @ 20° C: 0.61 kg O₂/kg</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>2. PHYSICAL AND CHEMICAL INFORMATION:</p> <p>SOLUBILITY: Completely miscible in water. Appearance: Clear, colorless to light straw colored liquid. (May be dyed blue at customer request)</p> <p>BOILING POINT: ~100°C</p> <p>EVAPORATION: No Data Available SPECIFIC GRAVITY: 1.08 - 1.13 VAPOR PRESSURE (20°C): No data available PERCENT VOLATILE (VOLUME %): No Data Available VAPOR DENSITY (AIR = 1): No Data Available VISCOSITY: 20 cP @ 20°C pH: 9 - 11</p> | <p>1.3. DISPOSAL CONSIDERATION: Based on information available to Cryotech Deicing Technology, this product is neither listed as a hazardous waste nor does it exhibit any of the characteristics that would cause it to be classified or disposed of as an RCRA hazardous waste. If product should spill or be otherwise unsuitable for normal deicing applications, it may be absorbed on suitable materials and disposed of in sanitary landfill unless state or local regulations prohibit such disposal.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>3. HAZARDOUS DECOMPOSITION PRODUCTS: None known.</p> <p>STABILITY: Stable.</p> <p>HAZARDOUS POLYMERIZATION: Polymerization will not occur.</p> <p>INCOMPATIBILITY: May react with strong acids or strong oxidizing agents, such as chlorates, nitrates, peroxides, etc. Avoid prolonged contact with reactive metals such as magnesium and zinc, especially in closed systems where hydrogen gas may accumulate over time.</p> <p>SPECIAL PRECAUTIONS: READ AND OBSERVE ALL PRECAUTIONS ON PRODUCT LABEL.</p> <p>Store away from strong oxidizing materials.</p> | <p>4. TRANSPORT INFORMATION: Not restricted under any transport regulations.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>5. REGULATORY INFORMATION:</p> <p>DOT SHIPPING NAME: Not designated as a hazardous material by the Federal DOT.</p> <p>DOT HAZARD CLASS: Not Applicable DOT IDENTIFICATION NUMBER: Not Applicable SARA 311 CATEGORIES:</p> <table style="width: 100%; border: none;"> <tr> <td>1. Immediate (Acute) Health Effects:</td> <td style="text-align: right;">Yes</td> </tr> <tr> <td>2. Delayed (Chronic) Health Effects:</td> <td style="text-align: right;">No</td> </tr> <tr> <td>3. Fire Hazard:</td> <td style="text-align: right;">No</td> </tr> <tr> <td>4. Sudden Release of Pressure Hazard:</td> <td style="text-align: right;">No</td> </tr> <tr> <td>5. Reactivity Hazard:</td> <td style="text-align: right;">No</td> </tr> </table> <p>REGULATORY LISTS SEARCHED:</p> <table style="width: 100%; border: none;"> <tr> <td>01 = SARA 313</td> <td>02 = MASS RTK</td> </tr> <tr> <td>03 = NTP Carcinogen</td> <td>04 = CA Prop. 65</td> </tr> <tr> <td>05 = MI 406</td> <td>06 = IARC Group 1</td> </tr> <tr> <td>07 = IARC Group 2A</td> <td>08 = IARC Group 2B</td> </tr> <tr> <td>09 = SARA 302/304</td> <td>10 = PA RTK</td> </tr> <tr> <td>11 = NJ RTK</td> <td>12 = CERCLA 302.4</td> </tr> <tr> <td>13 = MN RTK</td> <td>14 = ACGIH TLV</td> </tr> <tr> <td>15 = ACGIH STEL</td> <td>16 = ACGIH Calculated TLV</td> </tr> <tr> <td>17 = OSHATWA</td> <td>18 = OSHA STEL</td> </tr> <tr> <td>20 = EPA Carcinogen</td> <td>21 = TSCA Sect 4(e)</td> </tr> <tr> <td>22 = TSCA Sect 5(a)(e)(f)</td> <td>23 = TSCA Sect 6</td> </tr> <tr> <td>24 = TSCA Sect 12(b)</td> <td>25 = TSCA Sect 8(a)</td> </tr> <tr> <td>26 = TSCA Sect 8(d)</td> <td>28 = Canadian WHMIS</td> </tr> <tr> <td>29 = OSHA CEILING</td> <td></td> </tr> </table> <p>None of the components of this material are found on the regulatory lists indicated.</p> | 1. Immediate (Acute) Health Effects: | Yes | 2. Delayed (Chronic) Health Effects: | No | 3. Fire Hazard: | No | 4. Sudden Release of Pressure Hazard: | No | 5. Reactivity Hazard: | No | 01 = SARA 313 | 02 = MASS RTK | 03 = NTP Carcinogen | 04 = CA Prop. 65 | 05 = MI 406 | 06 = IARC Group 1 | 07 = IARC Group 2A | 08 = IARC Group 2B | 09 = SARA 302/304 | 10 = PA RTK | 11 = NJ RTK | 12 = CERCLA 302.4 | 13 = MN RTK | 14 = ACGIH TLV | 15 = ACGIH STEL | 16 = ACGIH Calculated TLV | 17 = OSHATWA | 18 = OSHA STEL | 20 = EPA Carcinogen | 21 = TSCA Sect 4(e) | 22 = TSCA Sect 5(a)(e)(f) | 23 = TSCA Sect 6 | 24 = TSCA Sect 12(b) | 25 = TSCA Sect 8(a) | 26 = TSCA Sect 8(d) | 28 = Canadian WHMIS | 29 = OSHA CEILING | | <p>6. OTHER INFORMATION: This Material Safety Data Sheet contains environmental, health and toxicology information for your employees. Please make sure this information is given to them. It also contains information to help you meet community right-to-know/emergency response reporting requirements under SARA Title III and many other laws. If you resell this product, this MSDS must be given to the buyer or the information incorporated in your MSDS. Discard any previous edition of this MSDS.</p> |
| 1. Immediate (Acute) Health Effects: | Yes | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 2. Delayed (Chronic) Health Effects: | No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 3. Fire Hazard: | No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 4. Sudden Release of Pressure Hazard: | No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 5. Reactivity Hazard: | No | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 01 = SARA 313 | 02 = MASS RTK | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 05 = MI 406 | 06 = IARC Group 1 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 07 = IARC Group 2A | 08 = IARC Group 2B | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 09 = SARA 302/304 | 10 = PA RTK | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 11 = NJ RTK | 12 = CERCLA 302.4 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| 20 = EPA Carcinogen | 21 = TSCA Sect 4(e) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 22 = TSCA Sect 5(a)(e)(f) | 23 = TSCA Sect 6 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 24 = TSCA Sect 12(b) | 25 = TSCA Sect 8(a) | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 26 = TSCA Sect 8(d) | 28 = Canadian WHMIS | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 29 = OSHA CEILING | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| <p>7. TOXICOLOGY DATA FROM RESEARCH:</p> <p>EYE IRRITATION: No product toxicology data available. The hazard evaluation was based on data from similar products.</p> <p>SKIN IRRITATION: No product toxicology data available. The hazard evaluation was based on data from similar products.</p> <p>DERMAL TOXICITY: No product toxicology data available. The hazard evaluation was based on data from similar products.</p> <p>RESPIRATORY/INHALATION: No product toxicology data available. The hazard evaluation was based on data from similar products.</p> <p>INGESTION: No product toxicology data available. The hazard evaluation was based on data from similar products.</p> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

The above information is accurate to the best of our knowledge. However, since data, safety standards, and government regulations are subject to change and the conditions of handling and use or misuse are beyond our control, Cryotech Deicing Technology, a Division of General Atomics International Services Corporation makes no warranty, either express or implied, with respect to the completeness or continuing accuracy of the information contained herein and disclaims all liability for reliance thereon. Cryotech Deicing Technology, a Division of General Atomics International Services Corporation assumes no responsibility for any injury or loss resulting from the use of the product described herein. User should satisfy himself that he has all current data relevant to his particular use.

OCTAGON PROCESS - AWAITING EMAIL REPLY

From: DAVID BOWMAN
To: AMEYERS@OCTAGONPROCESS.COM
Subject: POTTASIUM ACETATE RUNWAY DEICER AVAILABILITY

732 346-8000

Alex,

Per our phone conversation this morning please advise on the current availability of Pottassium Acetate runway deicer and when you think you may be in a position to begin filling requests for that product in the future.

Thanks for you quick response to my question.

Mail Envelope Properties (49354B16.50D : 247 : 39769)

Subject: POTTASIU ACETATE RUNWAY DEICER AVAILABILITY
Creation Date 12/2/2008 8:49:58 AM
From: DAVID BOWMAN

Created By: DAVID.BOWMAN@cityofchicago.org

| Recipients | Action | Date & Time |
|--|---------------|------------------------|
| OCTAGONPROCESS.COM AM AMEYERS (AMEYERS@OCTAGONPROCESS.COM) | Transferred | 12/2/2008 8:50:14 |

| Post Office | Delivered | Route |
|--------------------|------------------|--------------------|
| | | OCTAGONPROCESS.COM |

| Files | Size | Date & Time |
|--------------|-------------|------------------------|
| MESSAGE | 656 | 12/2/2008 8:49:58 AM |

Options

| | |
|-----------------------------|----------|
| Auto Delete: | No |
| Expiration Date: | None |
| Notify Recipients: | Yes |
| Priority: | Standard |
| ReplyRequested: | No |
| Return Notification: | None |

| | |
|---------------------------|----------|
| Concealed Subject: | No |
| Security: | Standard |

| | |
|-------------------------|--------------------|
| To Be Delivered: | Immediate |
| Status Tracking: | Delivered & Opened |



U.S. Department
of Transportation
Federal Aviation
Administration

Office of the Associate Administrator
for Airports

800 Independence Ave., SW
Washington, DC 20591

NOV 18 2008

Mr. Gregory Principato
President
Airports Council International - North America
1775 K Street, NW
Washington, DC 20006

Dear Mr. Principato:

Greg

This past October, the Federal Aviation Administration Technical Center completed pavement friction evaluations of several newer generation runway deicing fluids (RDFs). Preliminary results showed that when used for anti-icing, that is, on wet pavements free from ice/snow masses, the evaluated RDFs affected runway friction comparably to propylene glycol with urea and potassium acetate products.

Manufacturers of the tested RDFs have recently gained third-party lab certification on their products in accordance with Society of Automotive Engineers Aerospace Material Specification 1435, except for the one year Storage Stability Test (SST) requirement. The SST is necessary to determine if the remaining RDFs may be used the following winter season.

Given the shortage of available RDFs, and since the third-party lab certification reports show that all other material compatibility tests have passed, airports may use newer RDFs under the following conditions:

1. Only those RDFs that have been evaluated by the FAA Technical Center and have displayed comparable frictional effects to existing RDFs may be used. A list of those evaluated to date is enclosed.
2. At the end of the 2008/2009 winter season, airport operators must store unused RDFs until the SST is complete. At that point, RDFs passing the SST can be used for the following winter seasons. The failing RDFs cannot be used after long-term storage.

Sincerely,

D. Kirk Shaffer
Associate Administrator
for Airports

Enclosure

FAA Technical Center Runway Friction Tested of Runway Deicing Fluids (RDFs)

Currently available RDFs that were tested:

Octagon Process Inc. product trade names:

Octagon RD-1426 (Propylene Glycol + Urea)

Octamelt™ (Potassium Acetate + Propylene Glycol)

Cryotech product trade name:

E36 (Potassium Acetate)

New RDFs that demonstrated satisfactory friction characteristics under test conditions were:

Battelle product trade names:

RDF 6-2

RDF 6-3

RDF 6-3

RDF 6-4

RDF 6-12 (all are a Polyol/Organic salt)

Cryotech product trade names:

NX360 (aqueous acetate based solution + Sodium Acetate)

XT360 (aqueous bio-based solution)

FMC Corporation product trade name:

LithMelt™ (Potassium Acetate + Lithium)



U.S. Department
of Transportation
**Federal Aviation
Administration**

Office of the Associate Administrator
for Airports

800 Independence Ave., SW
Washington, DC 20591

October 10, 2008

Dear Industry Representative:

We have learned of a potential shortage of potassium acetate-based runway deicer fluid for the 2008/2009 winter season. A leading manufacturer of potassium acetate runway deicers has recently said that the availability of its E36 [trade name] will be significantly limited for the 2008/2009 winter season. This situation is a direct result of the lack of raw materials used in the manufacturing of any potassium acetate product.

Raw materials are in short supply due to an ongoing mine strike in Canada. One leading manufacturer reported last year it produced **9 million gallons** of E36 for the 2007/2008 winter season, but will only produce around **2 million gallons** for this upcoming winter season. We have confirmed that all major North American potassium acetate manufacturers are having similar difficulties.

In light of this shortage, airports should develop contingency plans to deal with winter conditions without the use of potassium acetate-based fluid deicers/anti-icers.

I have enclosed an information paper that provides additional details and suggestions for alternative deicers/anti-icers. I would appreciate if you would share this information with your members.

Sincerely,

Original Signed By:
D. Kirk Shaffer

D. Kirk Shaffer
Associate Administrator
for Airports

Enclosure

**ISSUE PAPER
AAS-1**

Group: Federal Aviation Administration; Office of Airports

Purpose: The FAA has learned of a potential shortage of potassium acetate [KAc] based runway deicer fluid [RDF] for the 2008/09 winter season

Background and Current Issues:

- A leading manufacturer of KAc-based runway deicers, Cryotech Deicing Technology [CDT] headquartered in Fort Madison, Iowa, recently informed FAA that the availability of their E36 [trade name] will be significantly limited for the 2008/2009 winter season.
- This situation is a direct result of the lack of raw materials used in the manufacturing of any KAc-based product.
- Raw materials are in short supply due to an on-going mine strike in Canada. As reported by CDT, last year they produced **9 million gallons** of E36 for the 2007/2008 winter season, but will only produce around **3 million gallons** for this upcoming winter season.
- We have confirmed that the major North American KAc manufacturers are having similar difficulties. They include CDT, Old World Industries, Inc. (OWI), Clariant Corp., Octagon Process, Inc and Jarchem Industries, Inc. CDT and OWI are the prime vendors. (See vendor info and product listing.)
- KAc-based products are the main RDFs used in the U.S. As reported by CDT, if the mine strike ended today, the delivery of raw materials and the manufacturing of additional KAc-based products would still take a few months to alleviate this situation.
- As reported by the manufacturers, a similar winter season to last year could translate into the depletion of limited KAc by January or February.
- Three of the four leading manufacturers offer solid deicer materials, some not containing KAc. As reported, they have sufficient materials for existing customers and some limited supplies for new customers.
- Buyers could see increased prices at time of shipment, which will be based upon available finished product.

FAA Actions: Friction, aircraft compatibility, and storage testing

- As part of its continuing efforts to address additional/alternative deicing products, the FAA's Office of Airports will conduct testing of several runway deicer fluids that contain lesser amounts of KAc at Pease International Tradeport Airport in New Hampshire in late September. **NOTE: These tests were already planned prior to the FAA's notification the potential shortage of KAc-based runway deicers.**
- One fully tested fluid deicer will be commercially available using propylene glycol + urea. (Two of the five major vendors of KAc offer this product).

- Ethylene glycol [EG] products meeting the Society of Automotive Engineers (SAE) Aerospace Aviation Material Specification (AMS) 1435, *Fluid. Generic. Deicing/Anti-Icing-Runways and Taxiways*, require no further FAA actions for their usage.
- Products showing successful friction test results still need to pass aircraft compatibility tests under SAE AMS 1435.
 - All runway deicers must pass the friction testing and AMS 1435 specifications before being used on runways. In terms of SAE AMS 1435, one possible solution in an emergency situation is for the FAA to work with the Air Transport Association (ATA) to relax the one-year storage stability tests since airport operators can agree not use leftover RDF until it passes this test. Fortunately, most if not all of these products to be tested for friction are already in for evaluation under SAE AMS 1435, including the one-year test.
- We anticipate success of most, if not all of these products, thus offering additional alternatives to airport operators.
- The results of the friction tests are expected in mid October and will be broadly disseminated to our certificated airports via Cert Alert, and to our non-certificated airports via newsletters and bulletins through our industry partners such as The American Association of Airport Executives (AAAE), the Airports Council International (ACI), the Air Transport Association (ATA), the National Association of State Aviation Officials (NASAO), and the Airport Owners and Pilots Association (AOPA).

Alternative Products

Fluids

- FAA Advisory Circular (AC) 150/5200-30, *Airport Winter Safety and Operations* also recommends fluids such as propylene glycol [PG] and ethylene glycol [EG], which are products meeting SAE AMS 1435.
 - These recommendations include products that contain lesser amounts of KAc or none at all. For example, Cryotech BX36 has less KAc than E36, while some products combine PG+KAc, and products made of PG+urea have no KAc.
- We have been informed that sufficient supplies of PG+urea are available for existing and new customers.

Solids

- AC 150/5200-30 also recommends dry materials including, urea, sodium formate, and sodium acetate products that satisfy SAE AMS 1431, *Compound. Solid Deicing/Anti-Icing Runways and Taxiways*.
- Of the three dry materials, sodium acetate products are the most vulnerable to the shortage, however we have not received reports of shortages at this time.
- Use of sand should be as prescribed by AC 150/5200-30.

Contingency Back-Up Planning- Airport operators that rely on KAc-based products should consider taking the following actions:

- Develop contingency plans; this could include the use of glycol-based fluid alternatives, solid runway deicers, and/or sand. Plans should include how to deal with wet snow, compacted snow, and icing events without the use of fluid deicers/anti-icers.
- Notify airport tenants, users, and Air Traffic Control of the shortage and the possible impacts to flight operations during this winter season.
- Involve an environmental specialist during the deicer selection process so the environmental runoff impacts are properly analyzed, documented and mitigated.

Runway Deicer Manufacturers and Commercially Sold Product Line

| Company and Trade Names | Fluid Potassium Acetate KAc | Fluid Potassium Acetate + propylene glycol KAc+PG | Fluid Propylene glycol+Urea PG+Urea | Fluid Potassium Formate KFor | Fluids New Entries & Developmental Products | Solid Sodium Acetate NAAC | Solid Sodium Formate NAFO | Airside Urea |
|--|-----------------------------------|---|-------------------------------------|---|---|-------------------------------|------------------------------|--------------|
| Clariant Corp North Carolina and Canada POC= Bryan McCreary Bryan.mccreary@clariant.com 704-822-2184 Cell 704-904-0799 | Safeway KA HOT SAE AMS 1435 | Does not produce the product | Does not produce the product | Safeway KF and Safeway KF HOT SAE AMS 1435 | | Does not produce the product | Safeway SF SAE AMS 1431 | |
| Cryotech Deicing Technology Fort Madison, Iowa POC=Keith L. Johnson -President & CEO keith.johnson@cryotech.com 800-346-7237 or 319-372-6012 www.cryotech.com | Cryotech E36 SAE AMS 1435 | Does not produce the product | Does not produce the product | Does not produce the product | Cryotech BX36 Susterra™ propanedio l, a new 100 percent renewably sourced product | Cryotech NAAC SAE AMS 1431 | Does not produce the product | |
| Octagon Process, Inc Edison, NJ POC=Alex Myers ameyers@octagonprocess.com 1-732-346-8000 | Octagon RD - 1435 SAE AMS 1435 | Octamelt SAE AMS 1435 | Octagon RD-1426 SAE AMS 1435 | Does not produce the product | | Does not produce the product | Does not produce the product | |
| Old World Industries, | Peak PA | Does not | ORD 2000 | Does not | | Does not | PEAK SF | |

| | | | | | | |
|--|--------------|---------------------|--------------|---------------------|---------------------|--------------|
| <p>Inc Northbrook, Ill POC= Mike Buschek mbuschek@oldworldind.com 1-847-559-2116 1-800-323-5440 www.oldworldind.com</p> | SAE AMS 1435 | produce the product | SAE AMS 1435 | produce the product | produce the product | SAE AMS 1431 |
|--|--------------|---------------------|--------------|---------------------|---------------------|--------------|

Other/Secondary Suppliers

| | | | | | | |
|--|--------------------------------------|---------------------------------------|--|-------------------------------------|-------------------------------------|-------------------------------------|
| <p>Jarchem Industries, Inc Newark, NJ POC=Mary Ellen Fitzgerald mfitzgerald@jarchem.com 973-344-0600x557 www.jarchem.com</p> | <p>Jargrip 2000 SAE AMS 1435</p> | <p>Clearway 1500 SAE AMS 1435</p> | <p>Clearway 1000 None for last 4-5 years</p> | <p>Does not produce the product</p> | <p>Does not produce the product</p> | <p>Does not produce the product</p> |
| <p>Basic Solutions Ontario, Canada POC Kelvin Williamson-Director Kelvin@basic-solutions.ca 905-562-0770 Cell-647-801-4109 www.basic-solutions.ca</p> | <p>Airport Ice Melt-Liquid</p> | <p>Does not produce the product</p> | <p>Does not produce the product</p> | <p>Does not produce the product</p> | <p>Does not produce the product</p> | <p>Airport Ice Melt-Solid</p> |

Clarification of Reference AC 150/5200-30:

The FAA only recommends runway de/anti-icer products for use on paved areas that have been certified by a 3rd party lab to meet SAE AMS 1431 [solids] or SAE AMS 1435 [fluids]. That is, we do not generate a listing of approved product trade names. Furthermore, we do not conduct SAE AMS certification testing nor do we specify which labs. Since this is a highly specialized technical field all product manufacturers send their products to 3-4 labs worldwide for 3rd party certification. We, through the FAA Technical Center, test frictional properties of proposed new products.

We recommend that airport operators only buy deicer products that have passed SAE AMS 1431 or 1435. They should also obtain a copy of the lab certification for their record. For airports certificated under 14 Code of Federal Regulations, Part 139, that copy should be placed in the Snow and Ice Control Plan.

ORD DEICER USAGE FOR 2008-2009

| PA | DATE | TANK READINGS | DATE | TOTAL USED | PA | DATE | ORDERED |
|----|------------|---------------|------------|------------|----|------------|---------|
| | 3/31/2008 | 351400 | 11/24/2008 | 2000 | | 10/30/2008 | 50000 |
| | 11/25/2008 | 348600 | 11/27/2008 | 1600 | | 12/1/2008 | 30000 |
| | 12/2/2008 | 256800 | 11/30/2008 | 3200 | | 12/1/2008 | 50000 |
| | | | 12/1/2008 | 112600 | | | |
| | | | 12/2/2008 | 7200 | | | |

TOTAL 126600

TOTAL AVAILABLE
TOTAL DEICER USED
TOTAL PA ORDERED (APPROX)

(INCLUDING DEICER IN TRUCKS 49,000 GALLONS)

ORD DEICER USAGE FOR 2008-2009

0

From: James Mclsaac
To: BOWMAN, DAVID, Ho, Benjamin, Smith, Christine, Glavin, Terrence
Date: 12/2/2008 1:13:37 PM
Subject: Re: Request for sole source amendment - Old World deicer - add PA to contract

David_ please leave a. Contact number asap so the sole source board can you. Thanks

-----Original Message-----

From: DAVID BOWMAN
Cc: Klein, Margaret <mgklein@cityofchicago.org>
To: Ho, Benjamin <benjamin.ho@cityofchicago.org>
To: Smith, Christine <csmith@cityofchicago.org>
To: Glavin, Terrence <Terrence.Glavin@cityofchicago.org>
Cc: Mclsaac, James <james.mcisaac@cityofchicago.org>

Sent: 12/2/2008 1:08:08 PM

Subject: Request for sole source amendment - Old World deicer - add PA to contract

Attached is the sole source request for the amendment to add pottasium acetate to PO 18624.

Please advise if there are any questions or if you need additional information regarding this request.

CC: Klein, Margaret

M/WBE DEFICIENCIES FOR PROJECT NO.: U-2-107

| | | | |
|--|---------------------|-------------------|--|
| Base Bid: | \$7,448,223.00 | | |
| MBE Commitment (26.69%): | \$1,987,805.84 | | |
| WBE Commitment (3.28%): | \$ 244,580.00 | | |
| Revised Contract Value: | \$6,544,626.64 | | |
| Revised MBE Commitment (26.69%): | \$1,746,760.85 | | |
| Revised WBE Commitment (3.28%): | \$ 214,663.75 | | |
| Substantiated Amount: | MBE: \$2,161,546.93 | WBE: \$ 66,245.94 | |
| <u>TOTAL DOLLAR SHORTFALLS:</u> | MBE: \$ 0.00 | WBE: \$148,417.81 | |
| Total % of Shortfall: | MBE: 0% | WBE: 2.27% | |

The following information is needed to substantiate compliance:

The waivers of lien for the following firms did not include a clear corporate and/or notary seal:

Ogden Ave. Materials - Waiver dated April 30, 2007

Invoices and/or Delivery tickets are needed for these firms:

Ogden Ave. Materials and E. King Trucking

Certified payrolls for these firms to substantiate compliance:

See Preliminary EEO Letter

Other:

Any and all partial waivers of lien and/or final waivers of lien for the scheduled M/WBEs affixed with corporate seals and/or notary stamps.



DEPARTMENT OF AVIATION

MEMORANDUM

Date: December 2, 2008

To: Montel M. Gayles
Chief Procurement Officer

Attention: James McIsaac
Deputy Procurement Officer

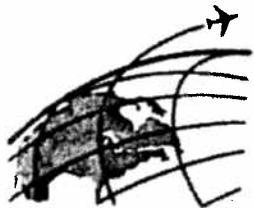
From: Richard L. Rodriguez
Commissioner

Subject: Request Approval for Amendment to
Non competitive Procurement Contract
Liquid Runway Deicer
Vendor: Old World Industries
PO Number: 18624
Specification Number: 69447
Expiration Date: 3/31/2010

The Department of Aviation (DOA) requests approval to modify the above referenced contract in order to allow for the ability to procure potassium acetate (PA) runway deicer. The subject contract for propylene glycol urea (PGU) was awarded in mid November and allowed the vendor a minimum of 30 days to procure the materials and equipment necessary to service the contract. Old World intends to be ready on or about December 20, 2008, however DOA is concerned its current inventory of runway deicer may not be sufficient to cover us until the PGU is available.

As there is no way to tell what the weather will be over the next three weeks (however long term forecasts predicts higher than average snow fall) DOA requires access to additional quantities of deicer to protect against inclement weather. We will only be obligated to purchase the product needed now and on an as available basis from Old World. Currently, Old World has approximately 350,000 gallons available.

If O'Hare were to run out of deicer it could jeopardize the safety of the traveling public and result in a runway/airport closure which would severely impact air traffic across the U.S. and entire World.



CHICAGO AIRPORT SYSTEM
Connecting People & Business



If you have any questions or need additional information regarding this request please contact David Bowman at 773-686-7089.

Thank you for your cooperation.

Procurement Type: Amendment to add line item and
increase vendor limit to \$21,256,500

Estimated Cost: \$4,021,500

Funding: 740 85 4005 0340 0340

User Deputy: William Palivos Phone: 686-3411

User Managing Deputy: Al Perez Phone: 894-1823

Reviewed by Angela Manning, Managing Deputy Commissioner *AMH*

Attachment: Sole Source Justification Package

Margaret Klein - PEAK PA Addendum

From: <jsaltzman@oldworldind.com>
To: <dbowman@ohare.com>
Date: 12/1/08 5:15 PM
Subject: PEAK PA Addendum
Attachments: ORD 2000 - Addendum V3.pdf; peak pa spec.pdf

Dave,

Please find attached offer for Peak PA submitted for inclusion as an addendum to our current contract as an interim supply solution until the PG Urea blend tanks are ready for service. Call with questions.

Joel

Old World Industries I, Ltd.
4065 Commercial Ave.
Northbrook, IL 60062
off ph: 847-559-2046
cell: 847-951-8791
fax: 847- 664-7046
e-mail: jsaltzman@oldworldind.com

OLD WORLD INDUSTRIES, INC.
1065 COMMERCIAL AVENUE E.
NORTHBROOK, ILLINOIS 60062-1831
847-559-2000 • FAX 847-559-1329

December 1, 2008

Mr. Dave Bowman
City of Chicago
Department of Procurement Services
Room 103, City Hall
121 North LaSalle Street
Chicago, IL 60602

Re: Propylene Glycol Urea Runway Deicer Supply Agreement (the "Agreement") for Chicago O'Hare International Airport ("O'Hare Airport") between the City of Chicago (the "City") and Old World Industries, Inc. ("Old World")
Specification No.: 69447
P.O. No.: 18624
Vendor No.: 1050120

Dear Mr. Bowman:

Old World is pleased to submit the following non-binding proposal to the City relative to the above-referenced Agreement. This proposal is in response to the City's request for access to an interim supply of runway deicer to supplement its current inventories until Old World is able to supply the runway deicer referenced in the Agreement.

This proposal would provide to O'Hare Airport (and Midway Airport if requested), first come-first served, subject-to-availability basis, varying quantities of Peak Potassium Acetate Runway Deicer Fluid ("PA Deicer") until Old World is able to manufacture and supply the ORD 2000 Propylene Glycol Urea Runway Deicer Fluid referenced in the Agreement.

This proposal is not intended to be and shall not be construed as a binding agreement. Any proposed obligations arising from this proposal shall be subject to and conditioned upon the execution, by and between the City and Old World of an actual amendment to the Agreement that is unconditionally acceptable to and accepted by both parties and executed by the appropriate authorized individuals.

In addition, this proposal and any amendment drafted there from is not intended to and shall not change the existing terms or conditions of the Agreement including Old World's MBE/WBE compliance commitments.

Mr. David Bowman
Department of Aviation
City of Chicago
December 1, 2008
Page 2 of 2

This proposal and any amendment drafted therefrom is also not intended to be and shall not be construed to be a minimum purchase obligation by the City for PA Deicer or a minimum sale and supply obligation by Old World of said product.

Proposed Non-Binding Commercial Terms:

Product: PEAK Potassium Acetate Runway Deicer (specification attached)

Price: \$11.49 per gallon, delivered to the Chicago-O'Hare International Airport or Chicago-Midway Airport

Payment Terms: Net 60 days from the date of invoice

Freight Terms: Freight to be prepaid by Old World

Lead Times: Old World would begin to load trucks no later than 24 hours after placement of product orders with deliveries to begin no later than 3 hours after initial pick-up

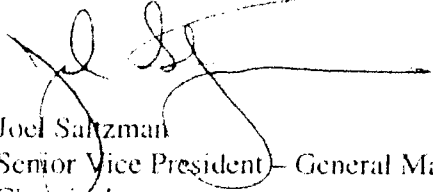
Quantity: Subject-to-availability on a first come-first served basis.

Term: Until supplies of PA Deicer are gone or until Old World terminates the availability of PA Deicer or until Old World is able to manufacture and supply the ORD 2000 Propylene Glycol Urea Runway Deicer referenced in the Agreement.

Thank you in advance for your consideration. Please contact me with any questions.

Sincerely,

Old World Industries, Inc.



Joel Saltzman
Senior Vice President - General Manager
Chemicals

**CITY OF CHICAGO
PRE-APPROVED
MODIFICATION / OVERRIDE REQUISITION**

Copy (Department)

| | |
|--|--|
| DELIVER TO: 208 AIRPORT OPERATIONS ADMIN T1 - MEZZ LEVEL, O'HARE CHICAGO, ILL 60666, IL | REQUISITION: 41044 For PO Number: 18624 PAGE: 1 DEPARTMENT: 85 - DEPT OF AVIATION PREPARER: David A Bowman NEEDED: PRE-APPROVED 12/1/2008 |
|--|--|

REQUISITION DESCRIPTION

REQUEST SOLE SOURCE AMENDMENT TO ADD LINE ITEM FOR POTTASIAM ACETATE LIQUID RUNWAY DEICER TO PO 18624 WITH OLD WORLD INDUSTRIES FOR LIQUID RUNWAY DEICER. INCREASE VENDOR LIMIT ~~\$2,872,500~~ TO ~~\$20,187,500~~.
 SPECIFICATION NUMBER: 69447
 Mod Reason: SCOPE CHANGE

\$4,021,500 \$21,256,500

COMMODITY INFORMATION

| LINE ITEM | QUANTITY | UOM | UNIT COST | TOTAL COST |
|---|------------|--------|-----------|------------|
| 1 1924612010 DE-ICERS, ROADWAY - POTASSIUM ACETATE | 250,000.00 | Gallon | 0.00 | 0.00 |

SUGGESTED VENDOR: OLD WORLD INDUSTRIES, INC. REQUESTED BY: David A Bowman

| DIST | BFY | FUND | COST CTR | APPR | ACCNT | ACTV | PROJECT | RPT CAT | GENR | L | FUTR | TOTAL COST |
|--------------------|-----|------|----------|------|--------|------|----------|---------|-------|---|------|------------|
| 1 | 008 | 0740 | 0854005 | 0340 | 220340 | 0000 | 00000000 | 000000 | 00000 | | 0000 | 0.00 |
| LINE TOTAL: | | | | | | | | | | | | 0.00 |

| LINE ITEM | QUANTITY | UOM | UNIT COST | TOTAL COST |
|--|--------------|-----|-----------|------------|
| 2 19246 INCREASE VENDOR LIMIT \$2,872,500 TO \$20,187,500 | <i>#####</i> | USD | 0.00 | 0.00 |

\$4,021,500 \$21,256,500

\$4,021,500

SUGGESTED VENDOR: OLD WORLD INDUSTRIES, INC. REQUESTED BY: David A Bowman

| DIST | BFY | FUND | COST CTR | APPR | ACCNT | ACTV | PROJECT | RPT CAT | GENR | L | FUTR | TOTAL COST |
|--------------------|-----|------|----------|------|--------|------|----------|---------|-------|---|------|------------|
| 1 | 008 | 0740 | 0854005 | 0340 | 220340 | 0000 | 00000000 | 000000 | 00000 | | 0000 | 0.00 |
| LINE TOTAL: | | | | | | | | | | | | 0.00 |

REQUISITION TOTAL: 0.00

Req has been updated in FMPS 12/2/08 and will print 12/3/08 with above corrections

Where a commodity is for a particular or unique use other than standard quality, grades, color, size or other characteristics, give details of how it will be and for what purpose. Requisitions prepared incorrectly will be returned to the using department.

corrections